

SECTION 3

STREETS

3.1 GENERAL

Street Improvements in the public rights-of-way shall be constructed by a Developer or City Contractor in conformance to these Improvement Standards:

1. Only a Developer or City Contractor with an appropriate license and required insurance may perform the work described herein.
2. Any Street Improvement damaged by the Developer or City Contractor shall be repaired by the Developer or City Contractor at his expense as required by the City.

3.2 GEOMETRIC DESIGN

The street widths shall conform to the standard details herein.

The design standard gutter slope shall not be less than 0.40% for new streets and not less than 0.20% for existing streets without special approval by the City. In problem areas, the gutter slope may be reduced to not less than 0.15% for residential areas.

The street cross slope grades shall be 2%. In special situations, the cross slope grade may be modified upon special approval by the City.

The street shall be designed to collect storm water at intersections whenever possible. Horizontal curves shall conform to the following criteria:

1. Minimum horizontal centerline curve radii shall be 250 feet.
2. Street intersections shall be as near right angles as practical. In no case shall the angle of intersection be less than 70 degrees. Streets located on opposite sides of an intersecting street shall have their centerline separated by not less than 10 feet.

3.3 STRUCTURAL DESIGN

The R-value design method used by the California Department of Transportation shall be used as a basis to determine the structural section of the streets.

A 10-year design life shall be used.

The Traffic Index (TI) used to determine the structural section shall be as follows:

STREET	T. I.
Major	10
Collector or Industrial	8
Residential	6
Cul-de-sac	5
Park Strip	5

In no case shall the ultimate thickness of Asphalt Concrete be less than 3 inches for major streets nor less than 2 inches for all other streets.

The minimum structural section shall be 4 inches Concrete over 4 inches of Class II Aggregate Base for any street or paved area.

3.4 CLEARING

Clearing and grubbing shall be done in accordance with Sections 16.1.01 thru 16-1.04 of the Standard Specifications.

3.5 EARTHWORK

Earthwork shall be performed as set forth in Sections 19-1.01, 19-1.02, 19-1.03, 19-2.01, 19-2.02 and 19-2.05 of the Standard Specifications.

All embankment of fill materials shall be placed and compacted in accordance with Section 19-5.02, 19-5.03 and, 19-5.04 of the Standard Specifications; except that the City Contractor or Developer will only be required to strip the original ground of vegetation and compact the top 6 inches of original ground in place to not less than 95% relative compaction in accordance with California Test Methods No. 216 and 231-1978, a nuclear density moisture gauge, before the fill is placed.

3.6 SUBGRADE

All clods shall be broken and all rocks, hard ribs and earth lumps over 2 1/2 inches in greatest dimension and other unsuitable material such as roots shall be removed from the job site. The sub-grade material shall be compacted to a firm, stable condition with approved equipment until a relative compaction of not less than 95% has been obtained to depth of 6 inches. Special Provisions may require a greater depth for 95% compaction.

The finished sub-grade shall not vary more than 0.05 foot above or below the planned grade at any point. Care shall be taken to obtain compaction around existing manholes and water valves.

Relative compaction shall be tested in accordance with California Test Methods No. 216 and 231.

3.7 AGGREGATE BASE

The aggregate base material shall conform to the requirements of Section 26 of the Standard Specifications for Class 2, 3/4 inch maximum combined grading.

Motor graders may be permitted to spread and shape the aggregate base materials, provided this method does not result in segregation of material. The aggregate base shall be maintained in a well mixed optimum moisture condition.

3.8 PRIME COAT

A liquid asphalt prime coat shall be applied in conformance with Section 39 of the Standard Specifications. The liquid asphalt prime coat grade shall be approved by the City.

When directed by the City, a sand cover shall be spread over the prime coat at approaches and side streets in order to maintain use. Before through traffic is permitted on the prime coat, all wet spots shall be completely covered with sand. All loose sand shall be completely removed from the treated areas before the placing of any surfacing material.

If, in the opinion of the City, conditions are such that this work is not feasible, the prime coat may be deleted. The Contractor shall issue an appropriate credit for such deletion.

3.9 ASPHALT CONCRETE

The asphalt concrete shall conform to the requirements of Section 39 of the Standard Specifications. Asphalt concrete used in all but the final course shall be Type B with 3/4 inch maximum, medium grading. Asphalt concrete used in the final course shall be Type B as follows:

Major, Collector and Industrial Streets - 3/4 inch maximum
Residential Streets - 1/2 inch maximum, medium grading

The asphalt grade shall be AR 4000 as specified by the latest revision of the Standard Specifications unless otherwise approved by the City.

3.10 ASPHALT PAINT BINDER

An asphalt paint binder shall be applied in conformance with Section 39-4.02 of the State Improvement Standards.

3.11 HEADER BOARDS

Header Boards shall be constructed to protect the edges of the asphalt concrete where streets are partially completed.

The boards shall be either pressure treated redwood or douglas fir with an American Wood Preservers Bureau Stamp indicating its use for ground contact and application of LP22 Water borne preservative or approved equal.

The boards shall be 2-inch x 4-inch minimum dimensioned of appropriate material.

3.12 CONCRETE

Portland cement concrete, unless otherwise specified in the Special Provisions, shall be Class B as defined in the Standard Specifications with a 28-day compressive strength of 2,500 pounds per square inch and a maximum slump of 3 inches. Concrete shall consist of Portland Cement, water and aggregate. Portland cement shall be Type II. Aggregate shall be washed before use and be free from any foreign matter.

The aggregates shall be graded to provide a plastic, workable mixture of maximum density with a maximum size aggregate of $\frac{3}{4}$ inches. The water shall be potable and no admixtures shall be used without prior approval by the City.

The Cement, water and aggregates shall be combined at the batch plant and be thoroughly mixed. No water shall be added to the mixture after leaving the batch plant without approval by the City.

The concrete shall be consolidated by tamping or vibrating. Concrete which has rock pockets or honey combing after curing shall be removed and replaced.

All concrete shall be cured in accordance with Section 90-7.01B of the Standard Specifications.

3.13 CURBS AND SIDEWALKS

Curbs and sidewalks shall be constructed in accordance with these Improvement Standards and Section 73 of the State Improvement Standards.

Sidewalk shall have a cross slope toward the curb face of 1/8-inch per foot minimum to 1/4-inch per foot maximum, unless otherwise approved by the City.

Traffic, parking and street name signs on city streets which require relocation shall be relocated by the City. Contractor shall request any such sign relocation in writing at least two working days prior to the date the relocation is needed. Utility poles which require relocation because of the work: shall be relocated by the respective utility company owning the poles. The Contractor shall provide sufficient notice to the subject utility

company to accomplish relocation by the date required. The Developer or City Contractor shall be responsible for protecting the work against damage and insuring the safety of the public.

Sidewalks which are required against the property line shall be placed 1-inch from the property line to provide space for lot corner monuments.

3.14 DRIVEWAY APPROACHES

Driveway approaches shall be constructed in accordance with these Improvement Standards and Sections 73 of the State Standards.

Approaches shall be located so they will not interfere with intersecting sidewalks, traffic signals, light poles, fire hydrants, or other public improvements unless specific approval is given by the City and the necessary adjustments to the improvements are accomplished without cost to the City.

3.15 ALLEYS

Commercial alleys shall be constructed in accordance with these Improvement Standards. All residential alleys shall be constructed with a minimum 2 inch crown and surfaced with 2 inches of asphalt concrete.

The concrete gutter for commercial alleys shall be constructed as required in Section 3.12 of these Improvement Standards.

The subgrade shall be constructed as required in Section 3.6 at these Improvement Standards.

The aggregate base for the commercial alley shall be constructed as required in Section 3.6 and 3.7 of these Improvement Standards and shall be 4" minimum in depth.

3.16 VALLEY GUTTERS

Valley Gutters designed to permit drainage across a street shall be constructed as shown on Drawing No. C.2, C.6 and C.7.

3.17 CURB RAMPS

Curb ramps shall be constructed at all intersections as shown on Drawings No. HC.1 and HC.2.

Ramps shall comply with Section 19956.5 of the State Health and Safety Code and the requirements of Title 24 and the ADA for accessibility.

3.18 RAISING UTILITY BOXES

Utility boxes and manholes shall be raised by the Developer or City Contractor to conform to these Improvement Standards. Utility boxes include, but are not limited to, sewer manholes, sewer lampholes, water valves, storm drain manholes and survey monuments.

Where existing utility boxes are in the work area their frames and covers shall be removed before subgrade compaction is made and a cover shall be placed to prevent dirt and loose material from entering the facility.

Base and surface material shall be placed over the covers, after which the frames and covers shall be set to finish grade as shown in Drawing No. ST.7.

3.19 TESTING

Material Testing required by the City for work done under the provisions of these Standards shall be provided by the Developer and submitted to the City for review. Testing required on City contract projects shall be provided by the City or shall be as indicated under the "Special Provisions" section in the specifications for the project.

On City contract projects, the City shall provide for the final passing test, per test site for compaction. All other compaction tests shall be paid for by the City Contractor.

3.20 ACCESS CONTROL WALL

Access control walls shall be constructed on all major thoroughfares and as required by the City.

Design of access control walls shall be submitted to the City for approval.

The wall shall be constructed shown on the Improvement Plans and a building permit shall be obtained by the Developer from the Chief Building Official prior to commencement of work on the wall.

3.21 STREET MONUMENTS

Street monuments shall be placed on the centerline of each street at the following locations:

1. Intersections
2. Beginnings and ends of curves

3. Changes of direction
4. Other points deemed necessary by the Engineer

The monuments shall consist of durable new material. They shall be ¾-inch O.D. x 30-inch long galvanized iron pipe or approved equal. They shall be set in a monument well as shown on Drawing No. M.1.

3.22 BOUNDARY MONUMENTS

Boundary monuments shall be placed on the exterior boundary of the subdivision at the following locations:

1. Changes of direction
2. Beginnings and ends of curves
3. Other points deemed necessary by the Engineer

Boundary monuments shall be of the same material as street monuments except that in unpaved areas the top shall be 1 foot below the finished grade.

3.23 BLOCK CORNER MONUMENTS

Corner monuments shall be placed at all block corners and alley corners.

Block corner monuments shall be of the same material as street monuments except the top shall be set 1 foot below the ground surface.

3.24 LOT CORNER MONUMENTS

Lot corner monuments shall be of the same material as street monuments except that in unpaved areas the top shall be 1 foot below the finished grade.

Lot corner monuments shall conform to the basic criteria as follows:

1. Lots shall have a monument at each corner except as otherwise provided by this section.
2. Lots that are created with zero back or side yards may have lot corners under the building foundations deleted except those corners that are block or alley corners.
3. Lots that are created as a part of a townhouse condominium development where realty is to be owned in fee by the individual lot owner shall be monument using a minimum of monument for each contiguous group of lots. Contiguous groups of lots containing more than 10 lots shall have additional monuments as required by the City.

4. Condominium airspace developments where realty is not to be owned in fee by the individual lot owner shall have exterior boundary monuments as required for Boundary Monuments.

3.25 BENCHMARK

A minimum of one benchmark shall be established (on a brass cap) within each subdivision of twenty lots or more. The location, elevation and datum, shall be shown on an as-built plan provided in mylar form (see Section 1.2). As possible the benchmark shall be set at the base of a street light standard. National Geodetic Survey vertical datum, NAVD 1988 shall be used. A copy of the level loop establishing the bench and its elevation shall be provided to the City for review and recordation.

3.26 PAVE-OUTS

Pave-outs shall be required whenever new curb and gutter is not constructed adjacent to existing pavement.

Pave-outs are to be designed according to the traffic index assigned to the classification of the adjacent street.

If the adjacent road is sub-standard according to its classification, the pave-out shall be constructed to centerline.

If the adjacent road conforms to standards, the pave-out may extend to the existing edge of pavement.

3.27 PARKING LOTS

All parking lots shall be constructed in accordance with these Standards, paved with a minimum of 2" Type B asphaltic concrete and have adequate drainage facilities to dispose of storm runoff within the boundaries of the property.

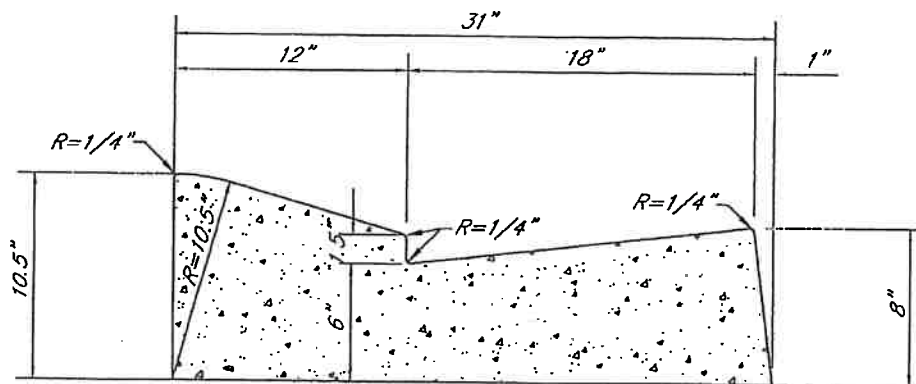
Surface slopes of parking spaces for the physically handicapped shall be the minimum possible and shall not exceed 1/4 in (6.35 mm) per foot (2.083% gradient) in any direction.

Each parking space reserved for the handicapped shall be identified by a permanently affixed reflectorized sign constructed of porcelain on steel, beaded test, or equal, displaying the International Symbol of Accessibility. The sign shall not be smaller than 70-in sq. (0.0452 m sq.) area and shall be centered at the interior end of the parking space at a minimum height of 36-in (914.4 mm) from the parking space finished grade, ground, or sidewalk.

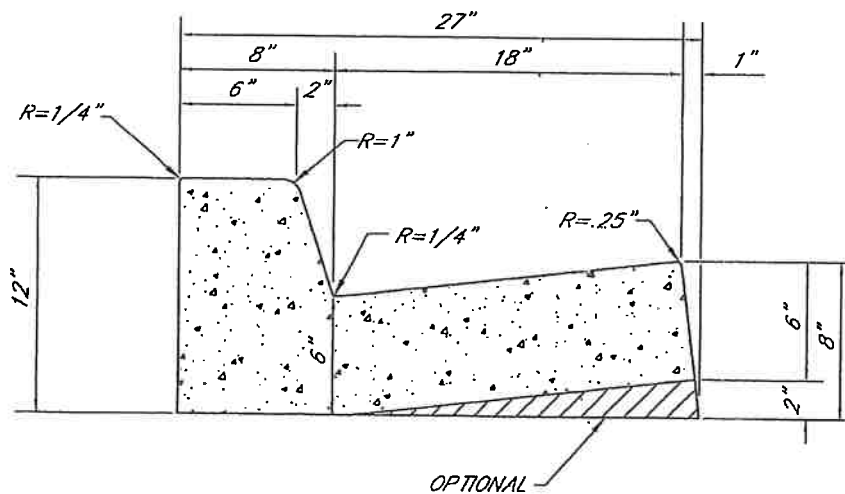
A sign shall also be posted, in a conspicuous place, at each entrance to the off-street parking facility, not less than 17-in x 22-in in height (431.8 x 558.8 mm), which clearly and conspicuously states the following:

"Unauthorized vehicles not displaying distinguishing placard or license plates issued for physically handicapped persons may be towed away at owner's expense. Towed vehicles may be reclaimed at _____ or by _____ telephoning _____." In addition to the above requirements, the surface of each parking place shall have a surface identification duplicating the symbol of accessibility in blue paint, at least 3 sq. ft. painted metal signs are acceptable whether they are steel or aluminum. Blue paint for the symbol of accessibility and curb markings should match color No. 15090 in the Federal Standard 595A as specified in Section 2-1720 of the regulations (similar to Royal Blue).

Entrances to and vertical clearances within parking structures shall have a minimum vertical clearance of 8 ft 2-in (2.489 m) where required for accessibility to handicapped parking spaces.



DRIVE-OVER CURB



VERTICAL CURB AND GUTTER



CURB AND GUTTER
DRIVE OVER AND VERTICAL

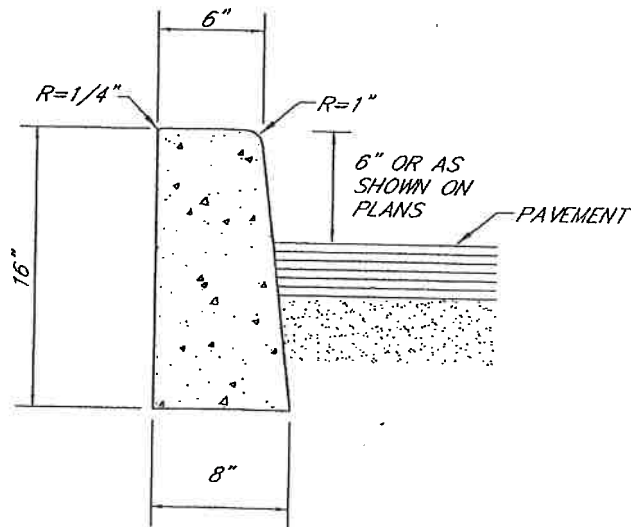
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DATE APPROVED: 8/13/07

CITY OF HUGHSON

STANDARD DETAIL

3-C.1

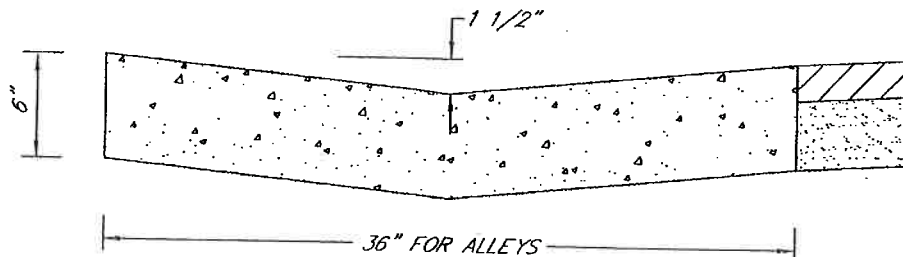


95% COMPACTION

NOTES:

1. A WEAKENED PLANE JOINT SHALL BE INSTALLED AT 10' INTERVALS
2. GUTTER FLOWLINES SHALL BE WATER TESTED FOR FLOW

MEDIAN CURB



95% COMPACTION

VALLEY GUTTER



MEDIAN CURB AND VALLEY GUTTER

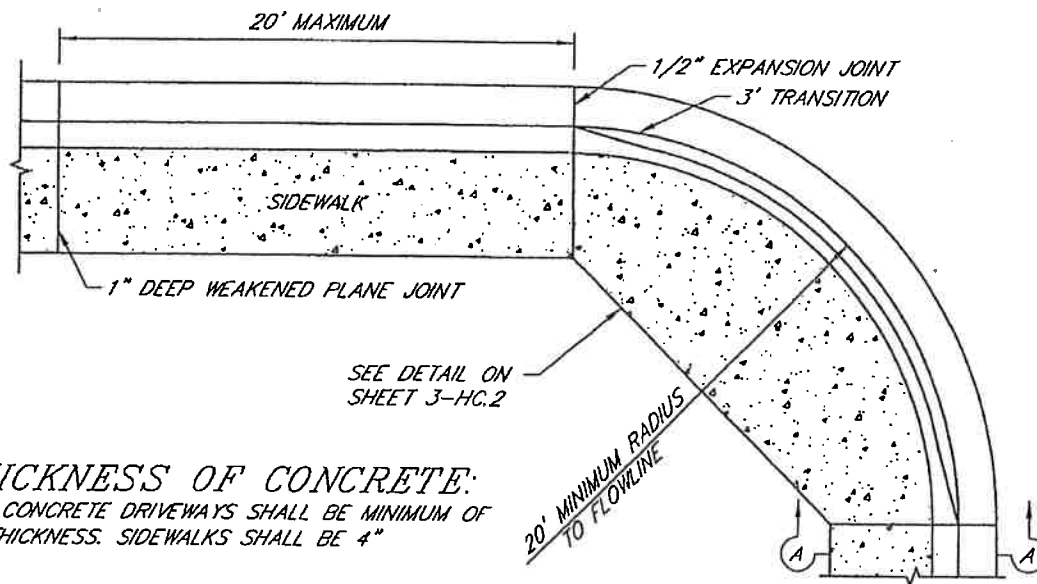
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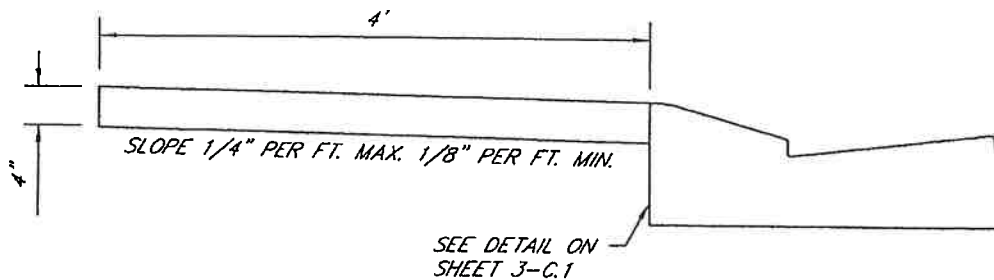
CITY OF HUGHSON

STANDARD DETAIL

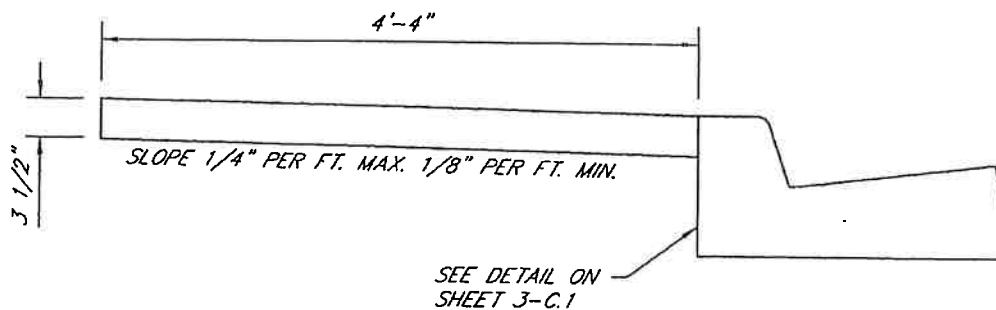
3-C.2



THICKNESS OF CONCRETE:
ALL CONCRETE DRIVEWAYS SHALL BE MINIMUM OF
6" THICKNESS. SIDEWALKS SHALL BE 4"



SECTION A-A DRIVE OVER CURB



VERTICAL FACE CURB & GUTTER



**CURB, GUTTER & SIDEWALK
PLAN AND SECTIONS**

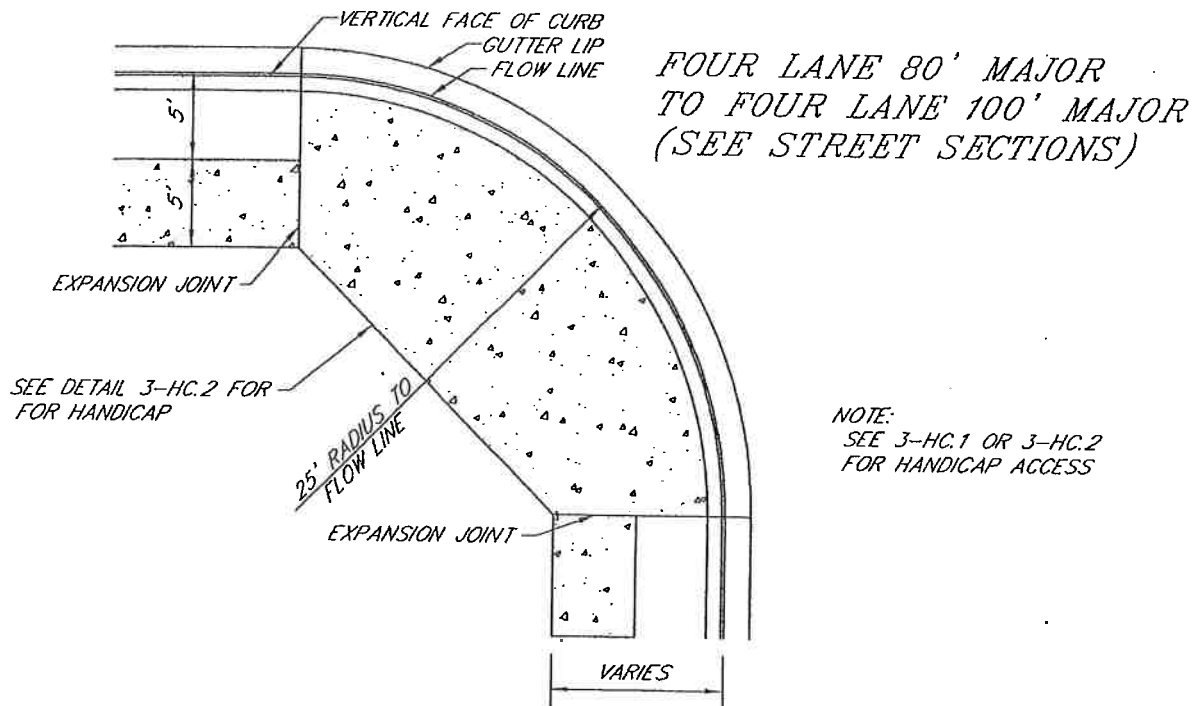
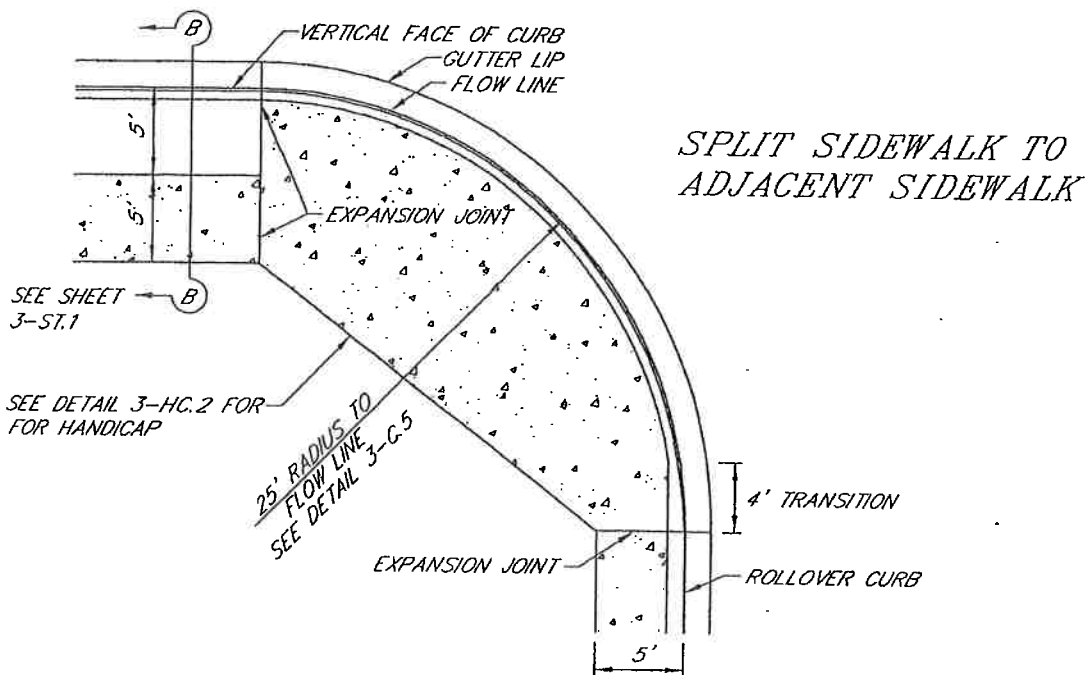
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DATE APPROVED: 5/1/04

CITY OF HUGHSON

STANDARD DETAIL

3-C.3



NOTE:
SEE 3-HC.1 OR 3-HC.2
FOR HANDICAP ACCESS



SIDEWALK RETURN DETAIL

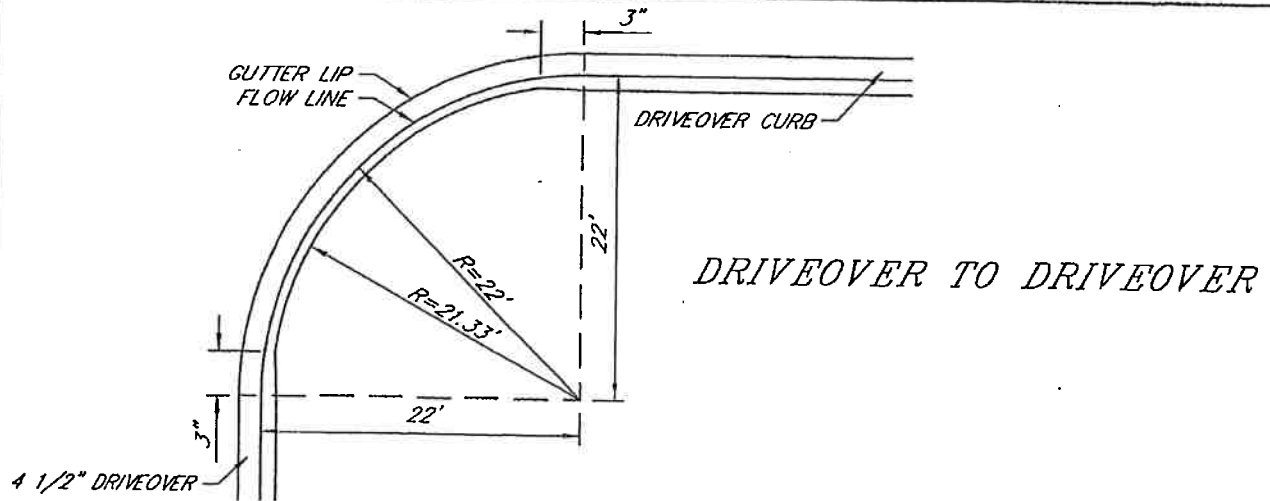
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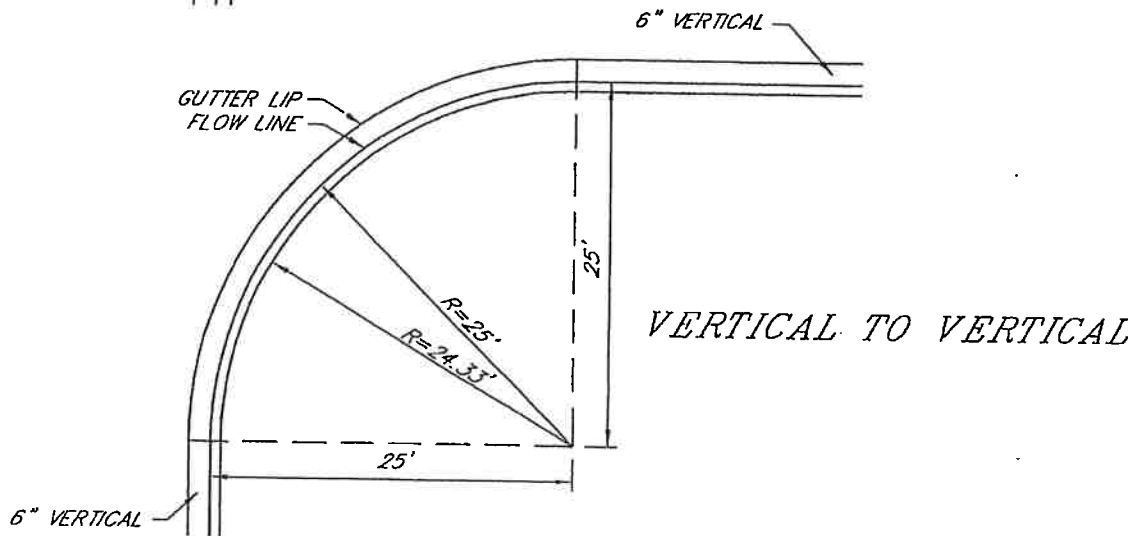
CITY OF HUGHSON

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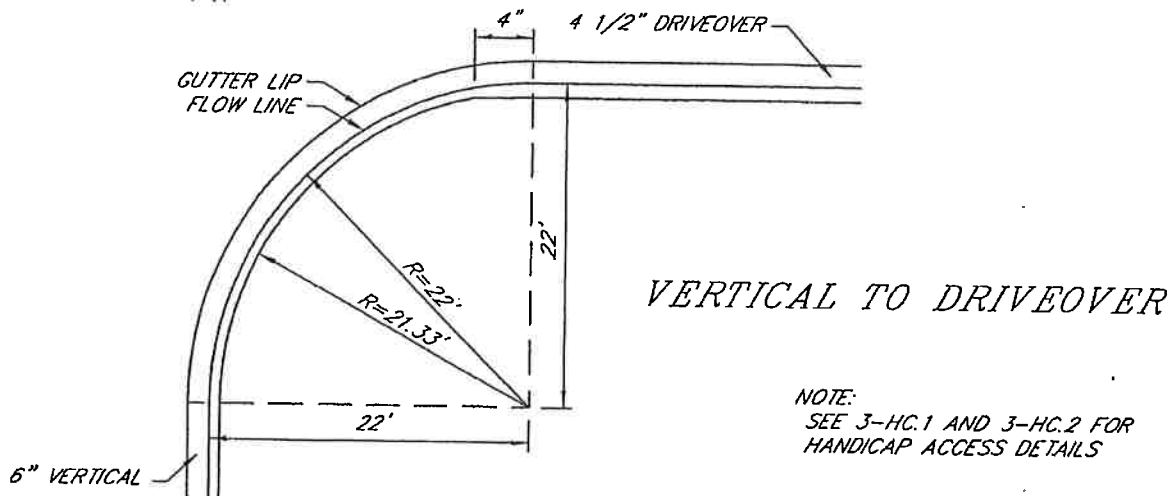
3-C.4



DRIVEOVER TO DRIVEOVER



VERTICAL TO VERTICAL



VERTICAL TO DRIVEOVER

NOTE:
SEE 3-HC.1 AND 3-HC.2 FOR
HANDICAP ACCESS DETAILS



CURB RETURNS
MINIMUM RADII

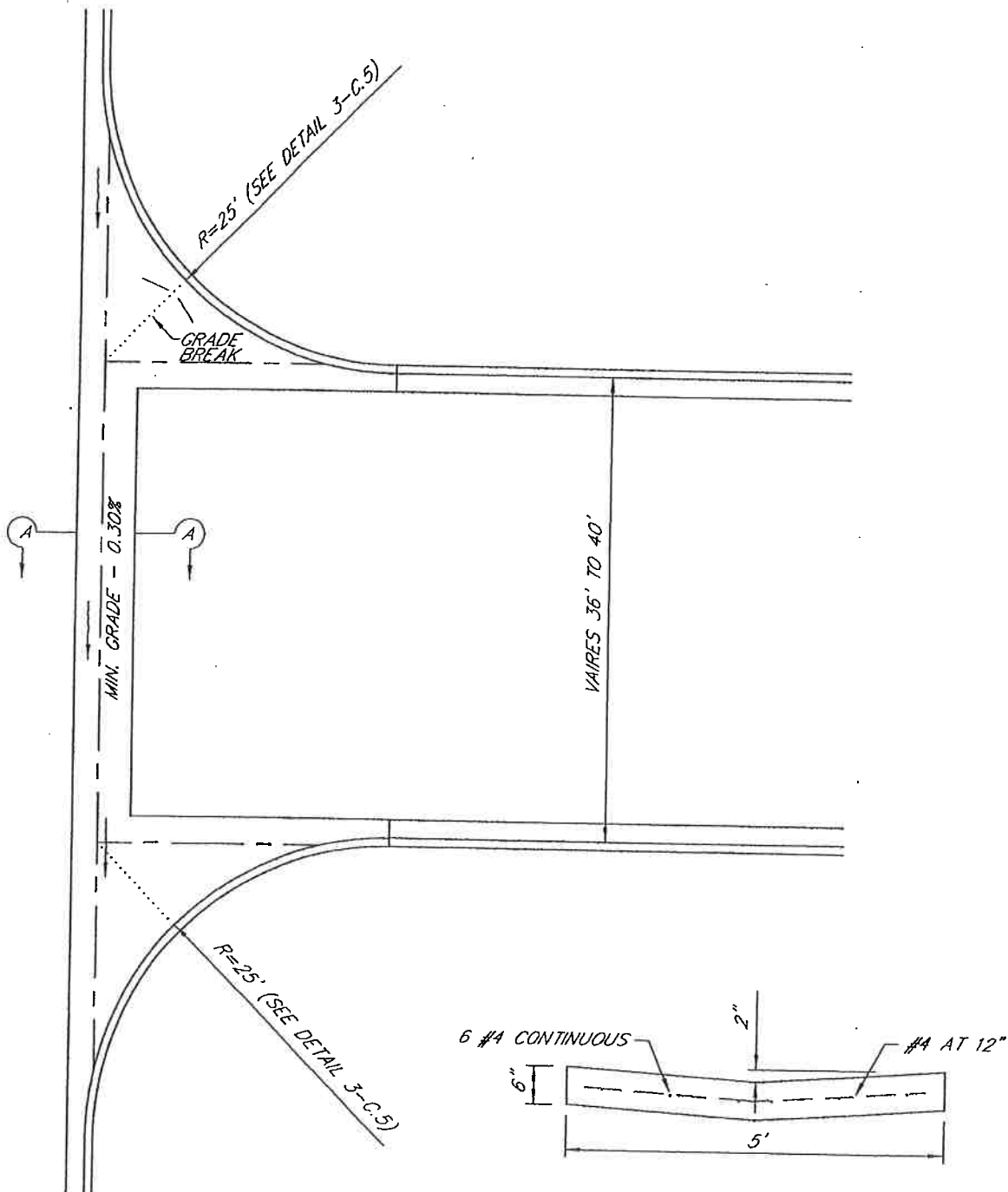
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CITY OF HUGHSON

STANDARD DETAIL

3-C.5



SECTION A-A



STANDARD VALLEY GUTTER
STREET CROSSING

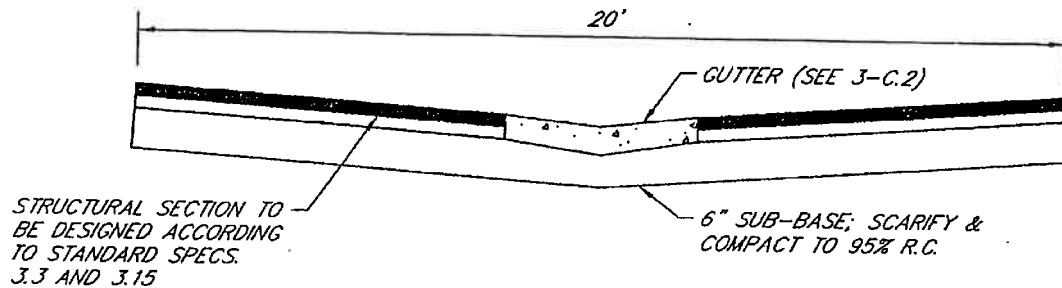
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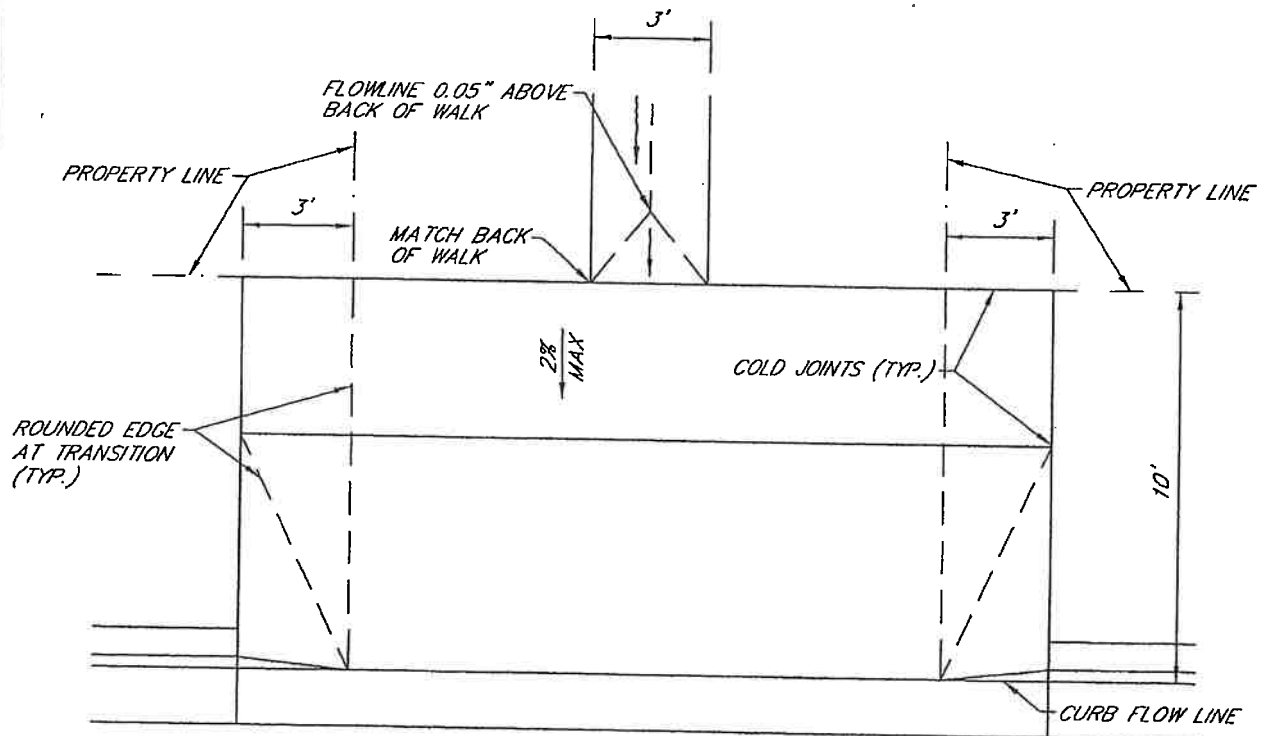
CITY OF HUGHSON

STANDARD DETAIL

3-C.6



ALLEY SECTION



PLAN

CONSTRUCT APPROACH PER DETAIL 3-C.9, WITH #4 REBAR, 12" O.C. BOTH DIRECTIONS.



COMMERCIAL ALLEY DRIVEWAY

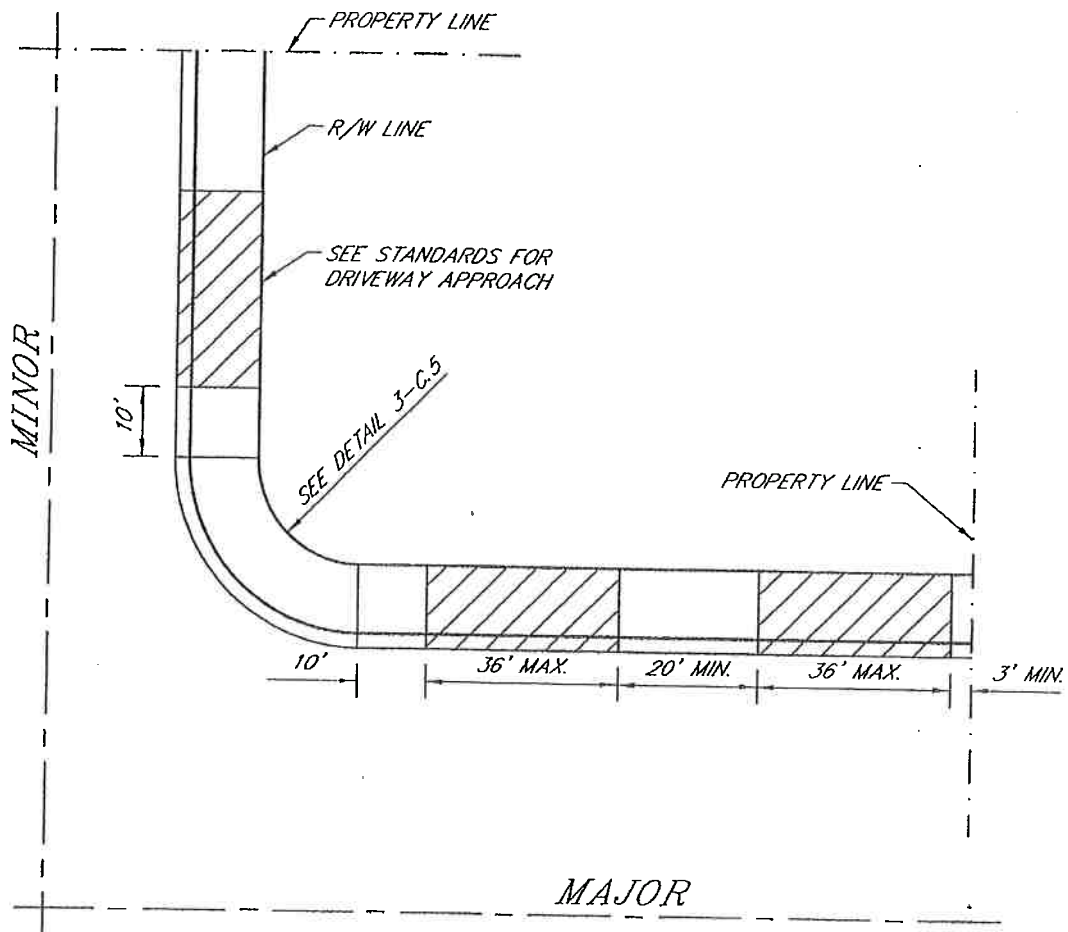
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CITY OF HUGHSON

STANDARD DETAIL

3-C.7



GENERAL NOTES:

1. FRONTAGE TO BE MEASURED ALONG THE STREET R/W LINE AND FROM THE INTERSECTION OF THE PROJECTED R/W TANGENTS ON CORNER LOTS.
2. DRIVEWAY FOR COMMERCIAL USES NOT PERMITTED WITHIN 10' FT. OF ADJACENT PROPERTY LINES AND/OR WITHIN 20' FT. OF INTERSECTION R/W TANGENTS WHOSE INTERIOR ANGLES OF INTERSECTION ARE GREATER THAN 70°. ALL OTHER CASES TO BE SUBMITTED TO THE DIRECTOR OF PUBLIC WORKS FOR INDIVIDUAL APPROVAL.
3. 20' FT. MINIMUM ALLOWABLE DISTANCE BETWEEN DRIVEWAY FOR LESS THAN 200 FT. FRONTAGE AND 40 FT. MINIMUM ALLOWABLE DISTANCES FOR FRONTAGE OF 200 FT. AND OVER.
4. 30 FT MAXIMUM DRIVEWAY WIDTH MEASURED AT R/W LINE. NO MORE THAN 40% OF FRONTAGE CAN BE DRIVEWAY.



DRIVEWAY LOCATION

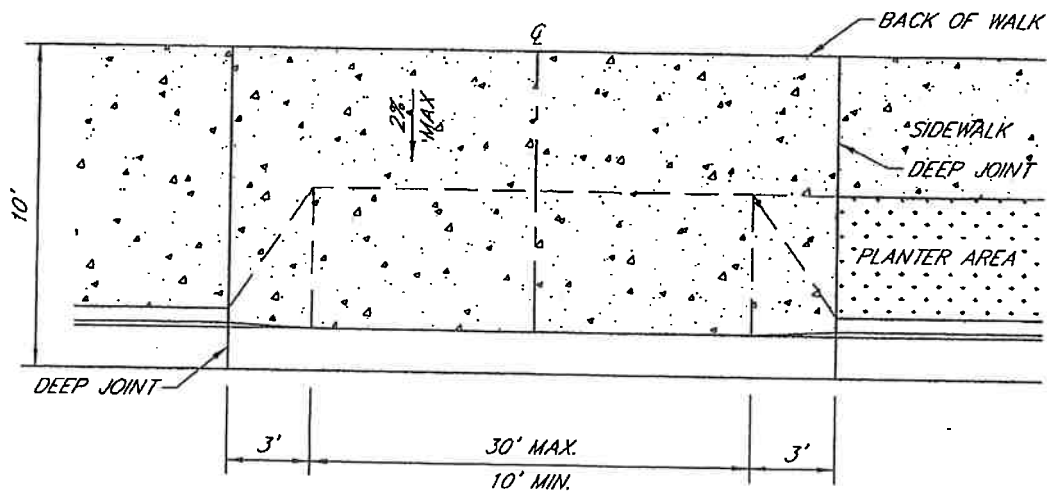
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CITY OF HUGHSON

STANDARD DETAIL

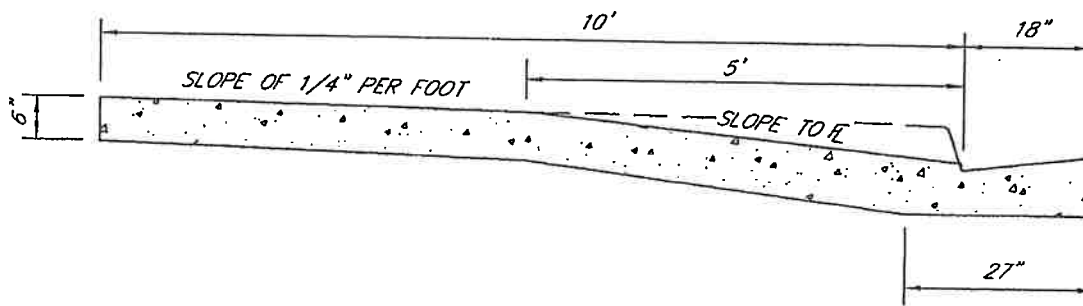
3-C.8



COMMERCIAL DRIVEWAY STANDARDS

NOTES:

1. DRIVEWAYS SHALL BE CONSTRUCTED OF CLASS "B" PORTLAND CEMENT CONCRETE.
2. 6" DRIVEWAY SECTION REQUIRED FOR BOTH VERTICAL & ROLLOVER CURBS AT ALLEYS.



CROSS-SECTION OF DRIVEWAY APRON



COMMERCIAL DRIVEWAY APRON

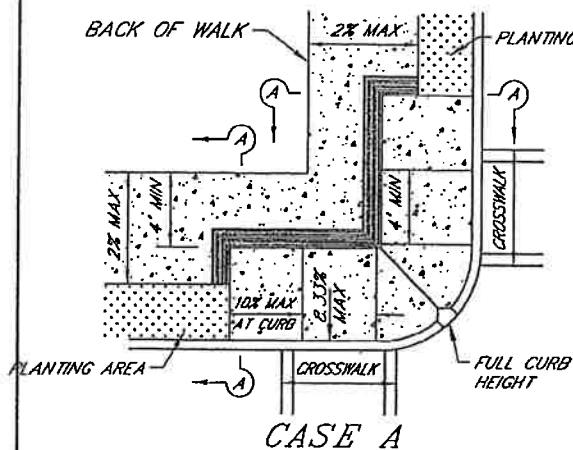
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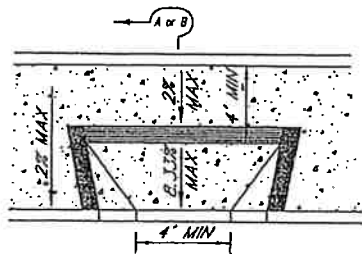
CITY OF HUGHSON

STANDARD DETAIL

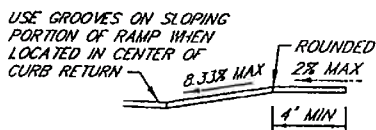
3-C.9



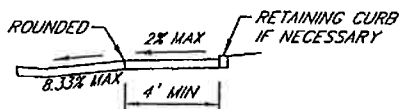
CASE A



CASE B



SECTION A-A



SECTION B-B



SECTION C-C

NOTES:

1. IF SIDEWALK IS LESS THAN 5' WIDE, THE FULL WIDTH OF THE SIDEWALK SHALL BE DEPRESSED AS SHOWN IN CASE C.
3. AS AN ALTERNATE TO CASE A, ONE RAMP MAY BE PLACED IN THE CENTER OF THE CURB RETURN AS IN CASE D.
4. WHEN RAMP IS LOCATED IN CENTER OF CURB RETURN, CROSSWALK CONFIGURATION MUST BE SIMILAR TO THAT SHOWN ON THE PLAN TO ACCOMMODATE WHEELCHAIRS.
5. IF PLANTING AREA IS EQUAL TO OR GREATER THAN RAMP LENGTH, RAMP SIDE SLOPE 'X' DISTANCE = 3 (SEE CASE F).
6. FOR CASES E AND F, THE LONGITUDINAL PORTION OF THE SIDEWALK MAY NEED TO BE DEPRESSED AS SHOWN IN CASE B.
7. IF LOCATED ON A CURVE THE SIDES OF THE RAMP NEED NOT BE PARALLEL, BUT THE MINIMUM WIDTH OF THE RAMP SHALL BE 4'.
8. THE BOTTOM OF THE RAMP SHALL HAVE A 1/2" LIP AT 45°.
9. SIDEWALK AND RAMP THICKNESS, "T", SHALL BE 3 1/2".
10. THE RAMP SHALL HAVE A 12' WIDE BORDER WITH 1/4" GROOVES APPROXIMATELY 3/4" O.C. SEE GROOVING DETAIL. THE SURFACE OF RAMP SHALL HAVE A TRANSVERSE BROOMED SURFACE TEXTURE ROUGHER THAN THE SURROUNDING SIDEWALK EXCEPT WHEN LOCATED IN CENTER OF CURB RETURN.
11. WHEN RAMP IS LOCATED IN CENTER OF CURB RETURN, IT SHALL BE GROOVED IN A HERRINGBONE PATTERN WITH 1/4" GROOVES APPROXIMATELY 1 1/2" O.C.S. SEE GROOVING DETAIL. GROOVES SHOULD BE ALIGNED PARALLEL TO CROSSWALK STRIPES TO DIRECT BLIND PEDESTRIANS INTO THE APPROPRIATE CROSSWALK.
12. RAMP SIDE SLOPE VARIES UNIFORMLY FROM A MAXIMUM OF UP TO 12.5% AT CURB TO CONFORM WITH LONGITUDINAL SIDEWALK SLOPE ADJACENT TO TOP OF THE RAMP, EXCEPT IN CASES C & F.
13. RETROFITS - WHEN A WHEELCHAIR RAMP IS ADDED TO AN EXISTING FACILITY, THE FOLLOWING CHANGES ARE PERMITTED.
 - (A) RAMP GRADE IN CASE MAY BE INCREASED TO 4
 - (B) OTHER RAMP GRADES MAY BE INCREASED TO A MAXIMUM OF 11.1%
 - (C) WHERE THE 4' PLATFORM IS NOT FEASIBLE, THE WIDTH MAY BE DECREASED TO 3'.
 - (D) THE PLATFORM MAY BE ELIMINATED IF THE GRADE DOES NOT EXCEED 8.33%



PEDESTRIAN RAMPS FOR THE HANDICAPPED

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CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

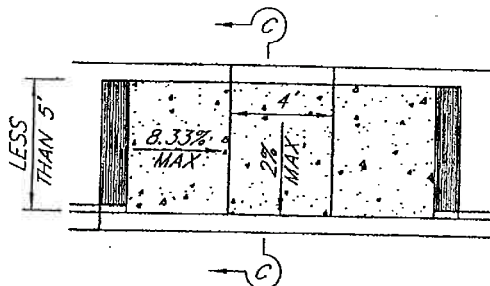
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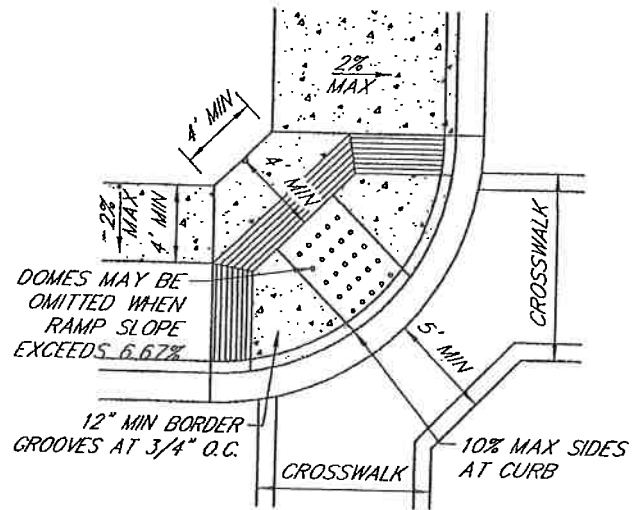
CITY OF HUGHSON

STANDARD DETAIL

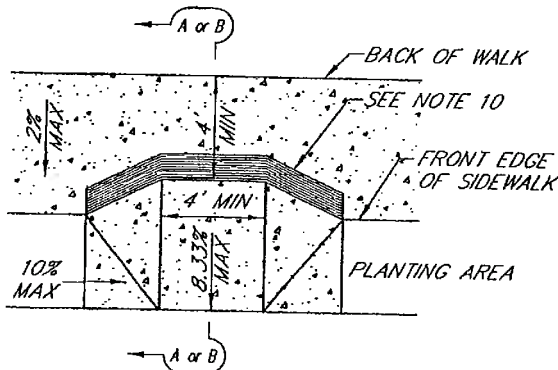
3-HC.1



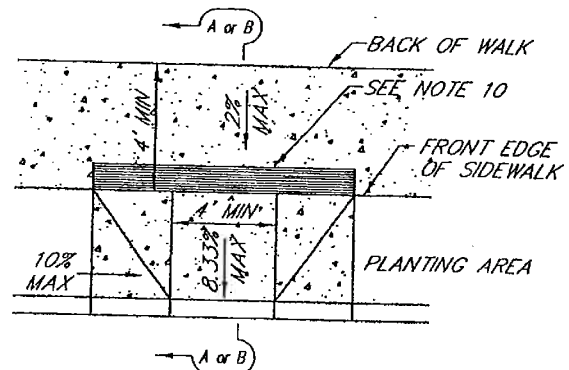
CASE C



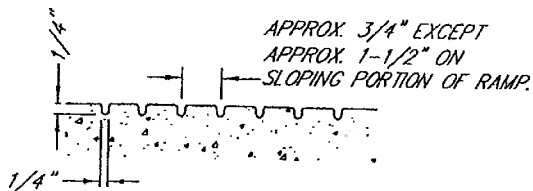
CASE D



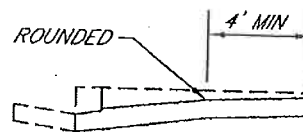
CASE E



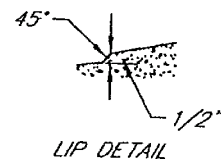
CASE F



CASE G



EXISTING CURB AND SIDEWALK
SEE SECTIONS ON
DETAIL SHEET 3-HC.2



PEDESTRIAN RAMPS FOR THE HANDICAPPED

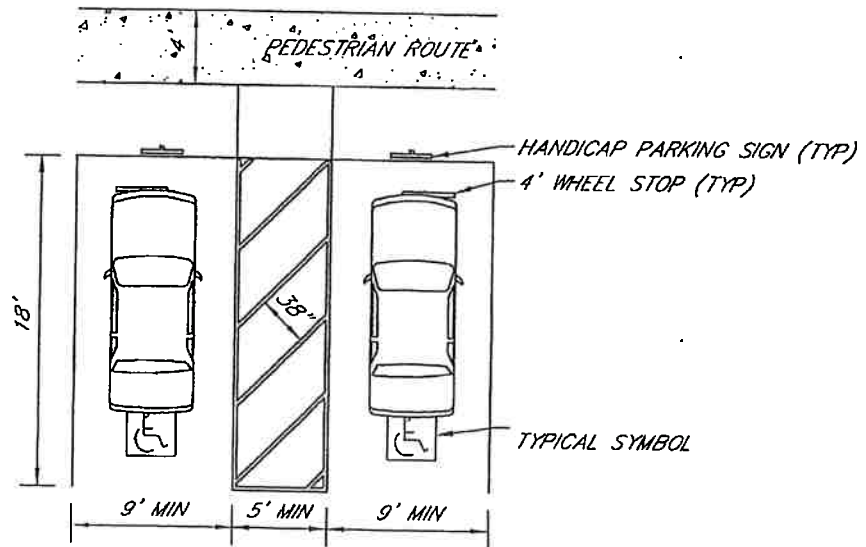
DRAWN BY: A.D.R.
CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

APPROVED BY: *[Signature]*
DATE APPROVED: 12/1/04

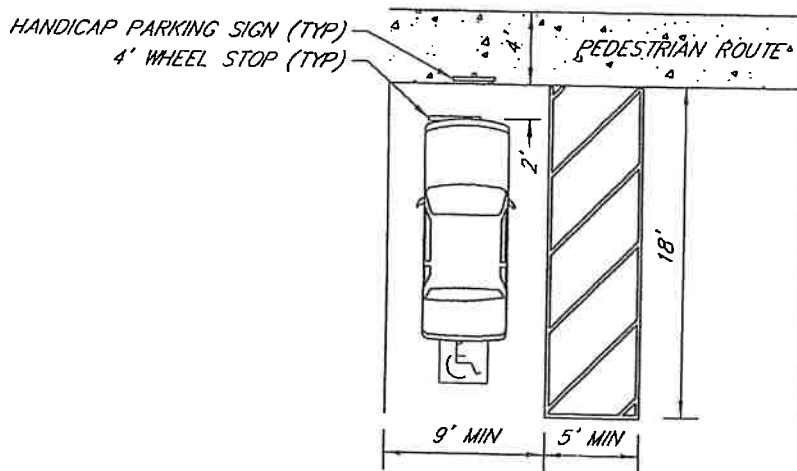
CITY OF HUGHSON

STANDARD DETAIL

3-HC.2



HANDICAPPED SPACES, DOUBLE TYPE



HANDICAPPED SPACES, SINGLE TYPE

	<h2 style="text-align: center;">HANDICAPPED PARKING REQUIREMENTS</h2>	DRAWN BY: A.D.R. CHECKED BY: R.H.H. SCALE: NONE DATE: 1/04
APPROVED BY: DATE APPROVED: 8/13/07	CITY OF HUGHSON	STANDARD DETAIL 3-HC.3

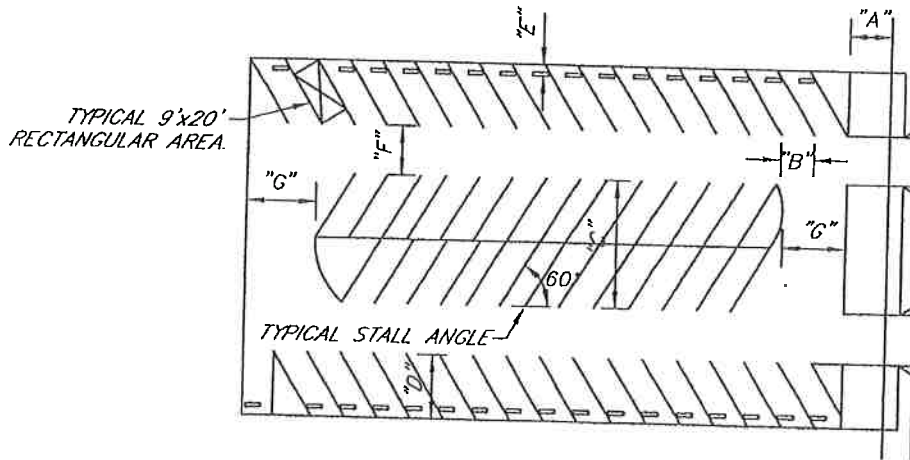
OFF STREET PARKING STANDARDS

SIZE AND ACCESS:

EACH OFF-STREET PARKING SPACE SHALL CONTAIN AT LEAST A RECTANGULAR AREA OF A MINIMUM WIDTH OF NINE (9) FEET AND A MINIMUM LENGTH OF EIGHTEEN (18') FEET. IF DESIRED, ONE OF EACH FIVE REQUIRED PARKING STALLS MAY BE SO SIZED AND POSTED TO BE USED FOR PARKING SMALL CARS. SMALL CAR STALLS SHALL CONTAIN A RECTANGULAR AREA A MINIMUM OF SEVEN AND ONE HALF (7.5) FEET IN WIDTH AND A MINIMUM OF FIFTEEN (15) FEET IN LENGTH. SEE NO. P-4 FOR HANDICAPPED PARKING REQUIREMENTS. ANY DRIVEWAY USED FOR BOTH INGRESS AND EGRESS TO AND FROM A PARKING LOT AND NOT DIRECTLY SERVING PARKING STALLS SHALL HAVE A MINIMUM WIDTH OF 20 FEET. ANY DRIVEWAY USED ONLY FOR EITHER INGRESS OR EGRESS TO OR FROM A PARKING LOT AND NOT DIRECTLY SERVING PARKING STALLS SHALL BE A MINIMUM OF 10 FEET IN WIDTH. ALL DRIVEWAYS CONNECTING PUBLIC RIGHTS-OF-WAY TO OFF-STREET PARKING LOTS SHALL COMPLY WITH CITY OF HUGHSON DEPARTMENT OF PUBLIC WORKS STANDARD SPECIFICATION.

THE FOLLOWING TYPICAL PARKING LOT DIAGRAM, PARKING STALL AND DRIVEWAY DIMENSION TABLE SHALL DETERMINE THE MINIMUM REQUIREMENTS FOR A PARKING LOT PLAN.

TYPICAL PARKING LOT DIAGRAM



THE REQUIRED FRONT SETBACK DIMENSION (A) IS AS SPECIFIED IN THE ORDINANCE CODE OF CITY OF HUGHSON FOR THE ZONING DISTRICT IN WHICH THE PARKING LOT IS TO BE LOCATED.

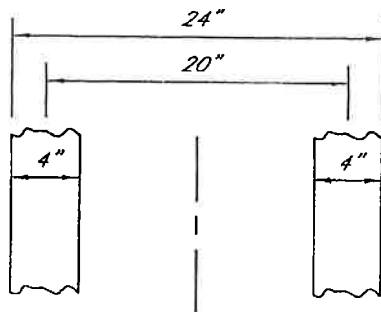
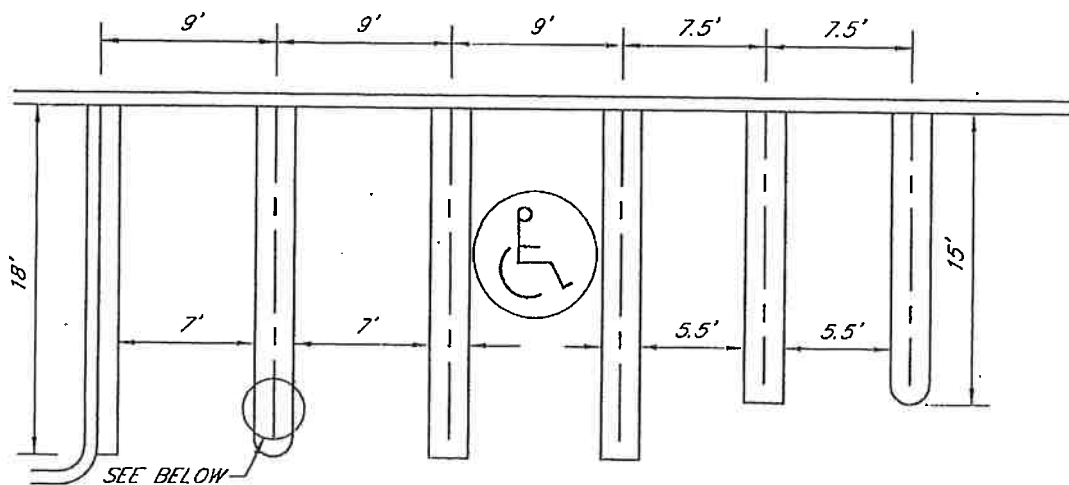
PARKING STALL AND DRIVEWAY DIMENSION

		45°	50°	55°	60°	65°	70°	75°	80°	85°	90°
STALL WIDTH	B	12.7'	11.8'	11.0'	10.4'	9.9'	9.6'	9.3'	9.1'	9.0'	9.0'
STALL DEPTH	D	20.5'	21.1'	21.6'	21.8'	21.9'	21.9'	21.7'	21.3'	20.7'	20.0'
STALL DEPTH	C	34.7'	36.4'	40.1'	40.7'	41.0'	40.7'	41.0'	41.0'	40.7'	40.0'
DRIVEWAY WIDTH	F	13.0'	15.0'	17.0'	19.0'	21.0'	23.0'	25.0'	27.0'	29.0'	31.0'

PLANS: THE PLAN OF THE PROPOSED PARKING AREA SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AT THE TIME OF THE APPLICATION FOR THE BUILDING PERMIT FOR THE BUILDING TO WHICH THE PARKING AREA IS ACCESSORY. THE PLANS SHALL CLEARLY INDICATE THE PROPOSED DEVELOPMENT, INCLUDING LOCATION, SIZE, SHAPE, DESIGN, CURB CUTS, LIGHTING, LANDSCAPING AND OTHER FEATURES AND APPURTENANCES OF THE PROPOSED PARKING AREA. SEE 3-HC.3 AND 3-HC.4 FOR HANDICAPPED PARKING REQUIREMENTS.

SURFACE OF PARKING AREA: PARKING AREAS SHALL BE PAVED WITH A MINIMUM OF TWO (2) INCHES OF ASPHALT SURFACING AND SHALL BE SO GRADED AND DESIGNED AS TO DISPOSE OF ALL SURFACE WATER IN ACCORDANCE WITH REQUIREMENTS OF THE CITY.

STRIPING OF PARKING AREA: PARKING AREAS SHALL BE MARKED BY EITHER STRIPING OR BUTTONS TO DELINEATE APPROVED STALLS AS SHOWN ON THE PLANS. SEE DIAGRAM BELOW.



STRIPING DETAIL

STRIPING SPECIFICATIONS:

STALLS SHALL BE DOUBLE STRIPED AS SHOWN IN THE DIAGRAM. PAINTED LINE WIDTH SHALL BE 4". IF BUTTONS ARE USED THEY SHALL BE 3-1/2 TO 4" IN DIAMETER, SPACED NO MORE THAN 3' ON CENTER. THE LINES SHALL BE LAID OUT AS PER DIAGRAM. EITHER SEMICIRCULAR CAP OR A STRAIGHT CAP MAY BE USED.

PAINT SPECIFICATIONS:

PAVEMENT MARKING PAINT SHALL BE READY-MIXED WITH PIGMENTS FULLY GROUND, MAINTAINING A SOFT PASTE CONSISTENCY. USE:

GLIDDEN: ULTRA HIDE WATERBASED TRAFFIC PAINT, MDF 7 MILS

SHERWIN WILLIAMS: SETFAST VINYL ACRYLIC WATERBORNTRAFFIC MARKING PAINT, MDF 7 MILS,

OR APPROVED EQUAL.



**PARKING STANDARDS
STRIPING DETAIL**

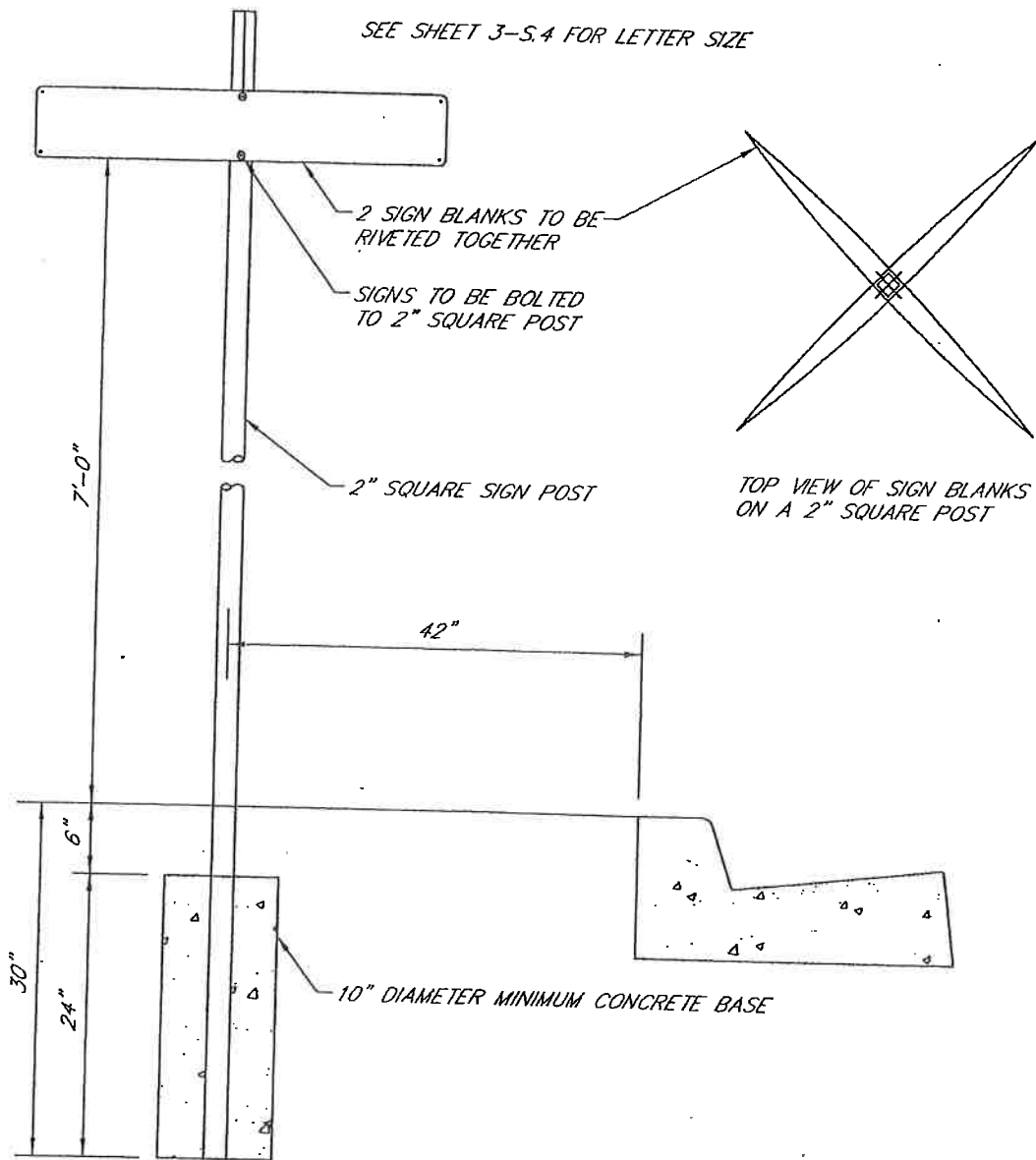
DRAWN BY: A.D.R.
CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

APPROVED BY: *[Signature]*
DATE APPROVED: 8/13/07

CITY OF HUGHSON

STANDARD DETAIL

3-P.2



STREET SIGN STANDARDS

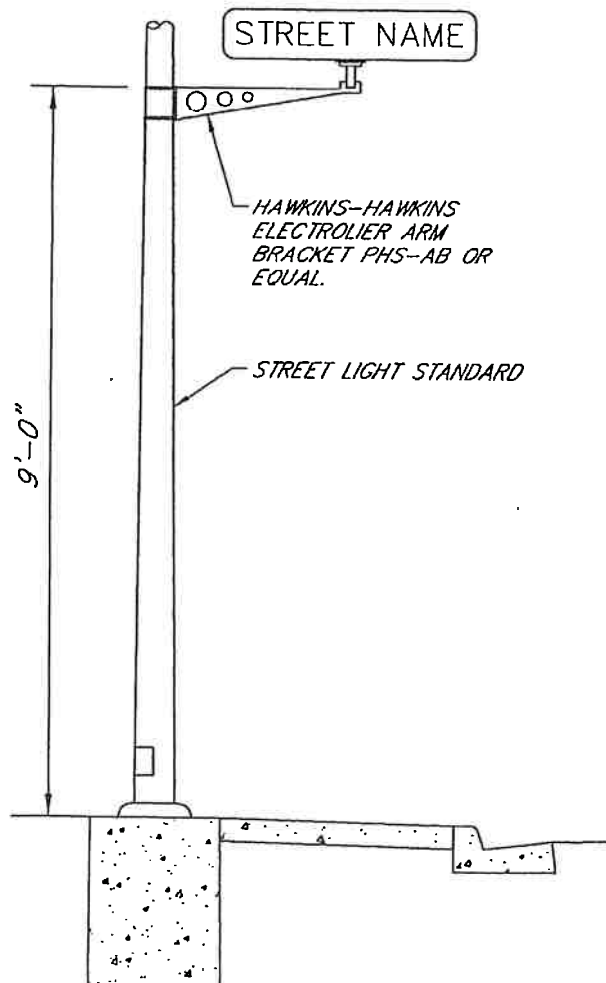
APPROVED BY: *[Signature]*
 DATE APPROVED: 5/1/04

CITY OF HUGHSON

DRAWN BY: A.D.R.
 CHECKED BY: R.H.H.
 SCALE: NONE
 DATE: 1/04

STANDARD DETAIL

3-S.1



HAWKINS-HAWKINS PARTS LIST: THE FOLLOWING LIST IS OF THE ITEMS NEEDED FOR EACH UNIT. THE DESCRIPTION AND PART NUMBER ARE FROM HAWKINS-HAWKINS CO. BERKELEY, CALIFORNIA, OR APPROVED EQUAL.

PART NO.	QUANTITY	DESCRIPTION
V-14-PHS	1 EA.	ELECTROLIER ARM BRACKET
M2-202	2 EA.	ELECTROLIER SIGN MOUNTING SET
V14-PHS-1024P	1 EA.	1/2" CENTER ROD 4 PLATE 13 1/2" ALUMINUM
V14-PHS-103	2 EA.	6" FEMALE SEPARATOR ALUMINUM .080"
V14-PHS-105	2 EA.	6" MALE SEPARATOR ALUMINUM .080"
V14-PHS-101	1 EA.	ZINC 1/2" THEFT-PROOF CAP NUT



ELECTROLIER S.N.S. ARM BRACKET

DRAWN BY: A.D.R.
CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

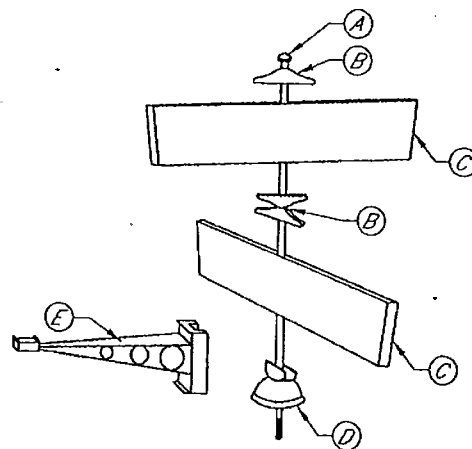
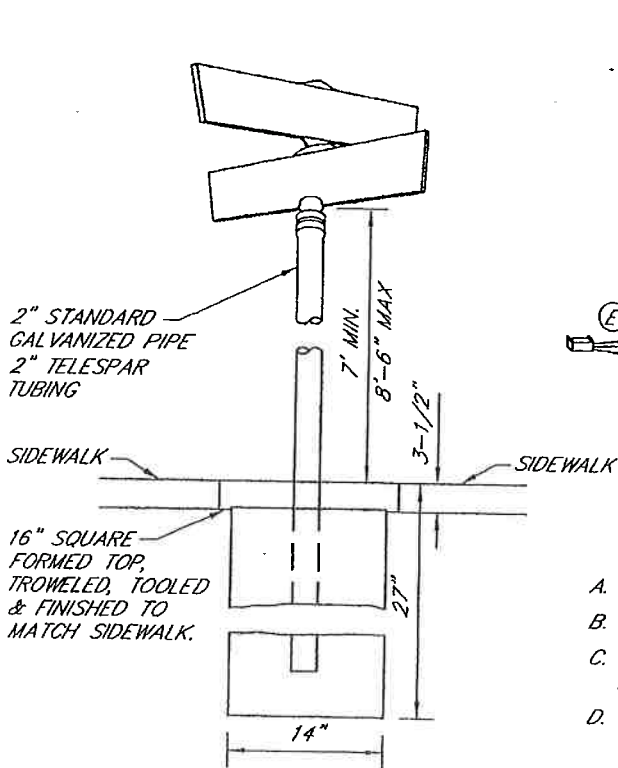
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DATE APPROVED: 8/18/17

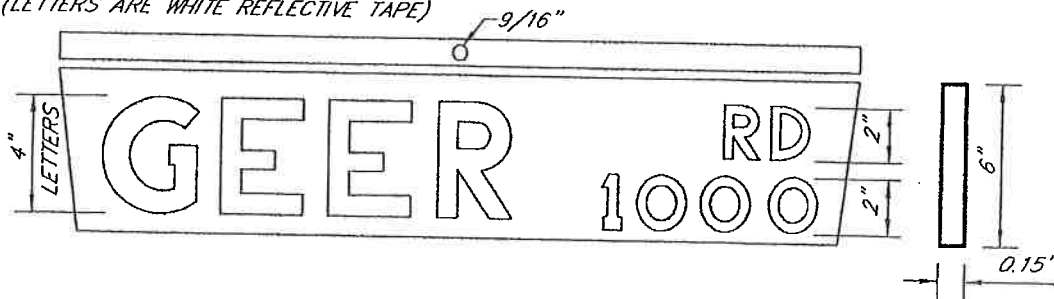
CITY OF HUGHSON

STANDARD DETAIL

3-S.2



WHITE LETTERS ON BLUE BACKGROUND
(LETTERS ARE WHITE REFLECTIVE TAPE)



NOTES:

- WHEN STREET SIGN IS LOCATED IN PLANTER AREA, TOP OF FOUNDATION SHALL BE 4" BELOW FINISH GRADE.
- STOP SIGN SHALL BE STANDARD SIZE AND COLOR. R-1, 24" MINIMUM.



STREET SIGN STANDARDS

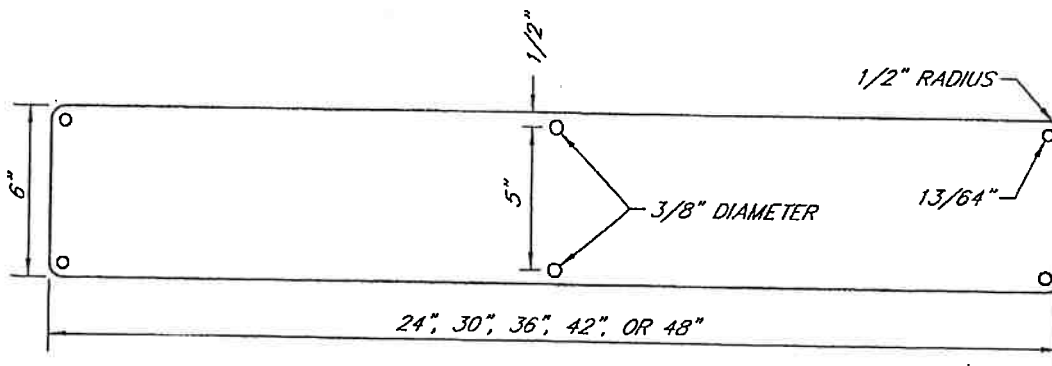
DRAWN BY: A.D.R.
CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

APPROVED BY: *[Signature]*
DATE APPROVED: 5/11/04

CITY OF HUGHSON

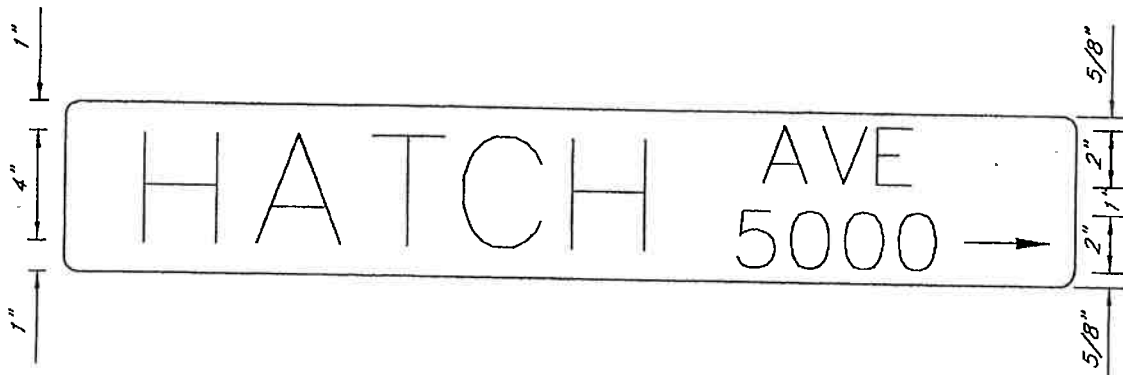
STANDARD DETAIL

3-S.3



1. ALL BLANKS TO BE .063 GAUGE ALUMINUM ALLOY (6061-TB OR 5155-H380, DEGREASED, AND TREATED WITH ALODINE 1200.
2. THE SIGN BLANKS SHALL CONFORM TO THE STANDARD B.P.R. SHAPES AND CORNER RADII, EXCEPT THAT HOLE PUNCHING, OR DRILLING, SHALL CONFORM TO THE HOLE SIZES AND LOCATIONS SHOWN HEREON. EACH CORNER SHALL HAVE A 13/64" DIAMETER HOLE.
3. ALL BLANKS SHALL BE 6" HIGH BY 24", 30", 36", 42", OR 48" WIDE. THE 3/8" HOLES SHALL BE CENTERED ON THE VERTICAL CENTERLINE OF EACH BLANK.

ALUMINUM SIGN BLANK



1. EACH SIGN FACE SHALL HAVE SUFFICIENT SPACING PROVIDED TO PERMIT APPLICATION OF A 4 DIGIT NUMBER AND ARROW.
2. NUMERICAL STREET NAMES SHALL BE SPELLED OUT INSTEAD OF USING THE NUMERAL LETTER TYPE ABBREVIATED LEGENDS.
3. ALL SIGN FACES TO BE "SCOTCHLITE" BRAND REFLECTIVE SHEETING-ENGINEER GRADE OR OF EQUAL QUALITY OR BETTER, WITH REFLECTIVE SILVER COPY ON REFLECTIVE GREEN BACKGROUND.

NAME FACE



STREET SIGN STANDARDS

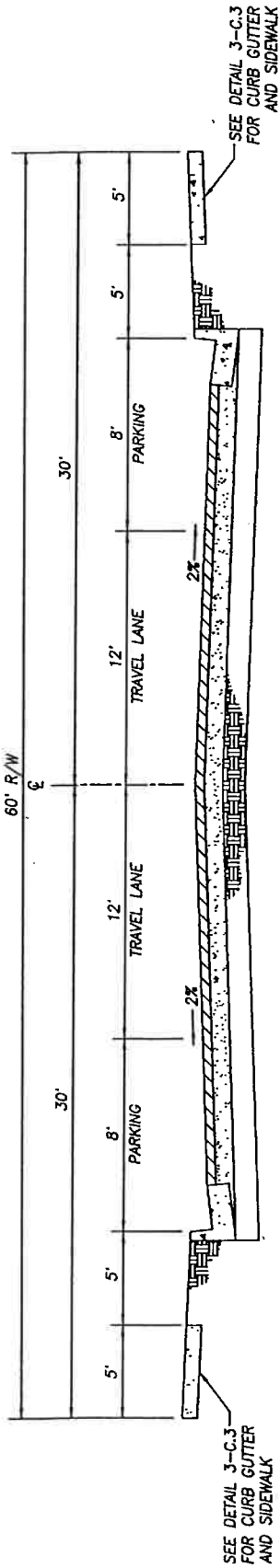
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CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

APPROVED BY: *[Signature]*
DATE APPROVED: 5/1/24

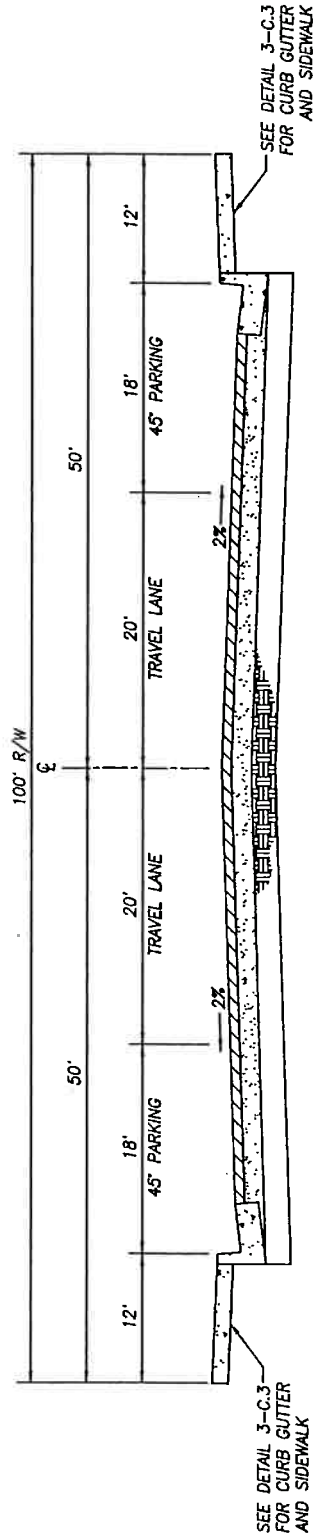
CITY OF HUGHSON

STANDARD DETAIL

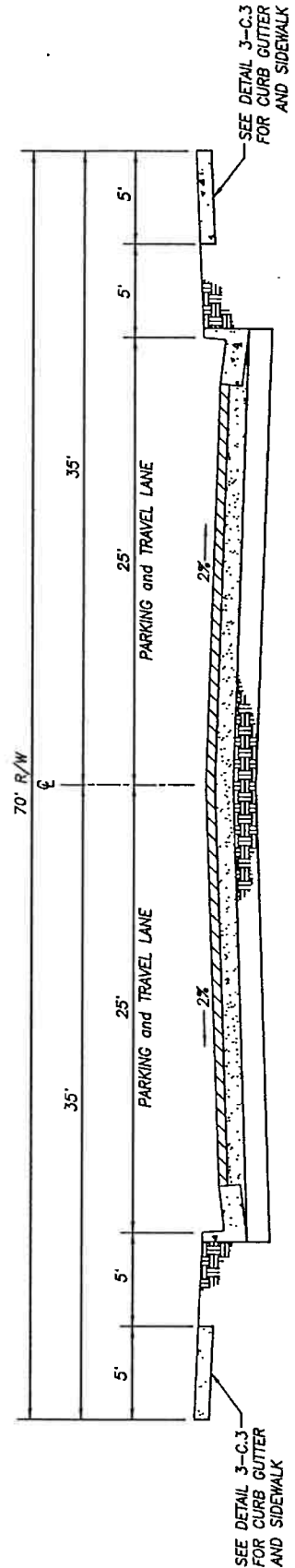
3-S.4



MINOR COLLECTOR STREET



DOWNTOWN COLLECTOR



INDUSTRIAL STREET



TYPICAL STREET SECTIONS COLLECTOR - MINOR, DOWNTOWN, & INDUSTRIAL

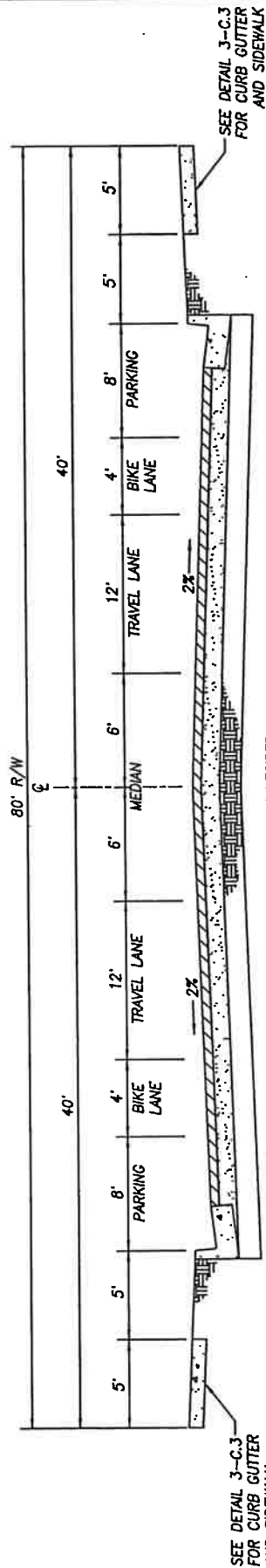
DRAWN BY: C.V.
CHECKED BY: P.K.
SCALE: NONE
DATE: 7/07

APPROVED BY: *[Signature]*
DATE APPROVED: 8/13/07

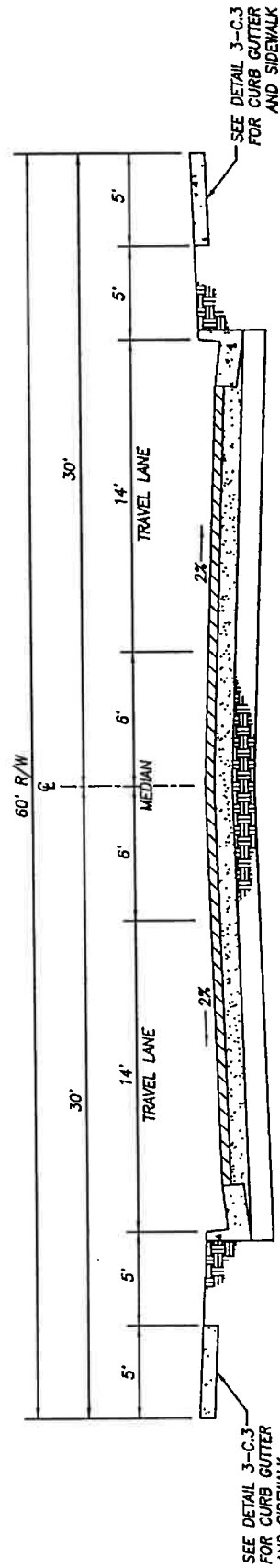
CITY OF HUGHSON

STANDARD DETAIL

3-ST.1.1



MAJOR COLLECTOR STREET



CONSTRAINED MAJOR COLLECTOR
(ON-STREET PARKING NOT PERMITTED)



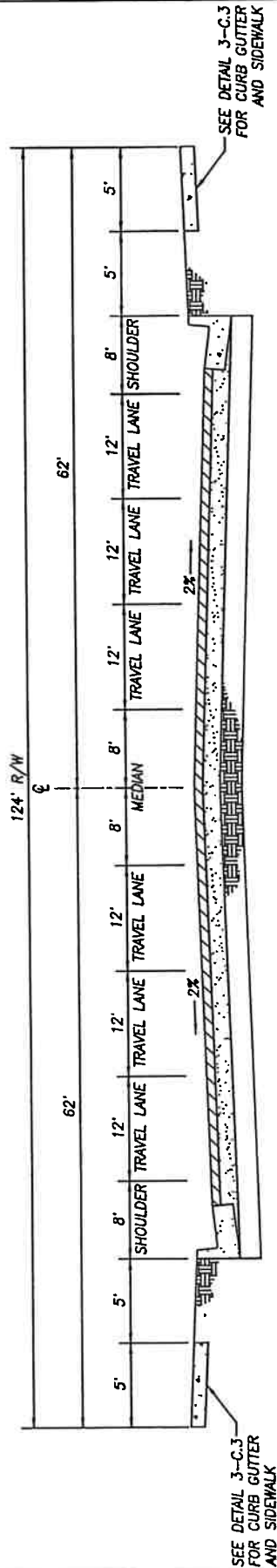
TYPICAL STREET SECTIONS COLLECTOR - MAJOR & CONSTRAINED MAJOR

DRAWN BY: C.V.
CHECKED BY: P.K.
SCALE: NONE
DATE: 7/07

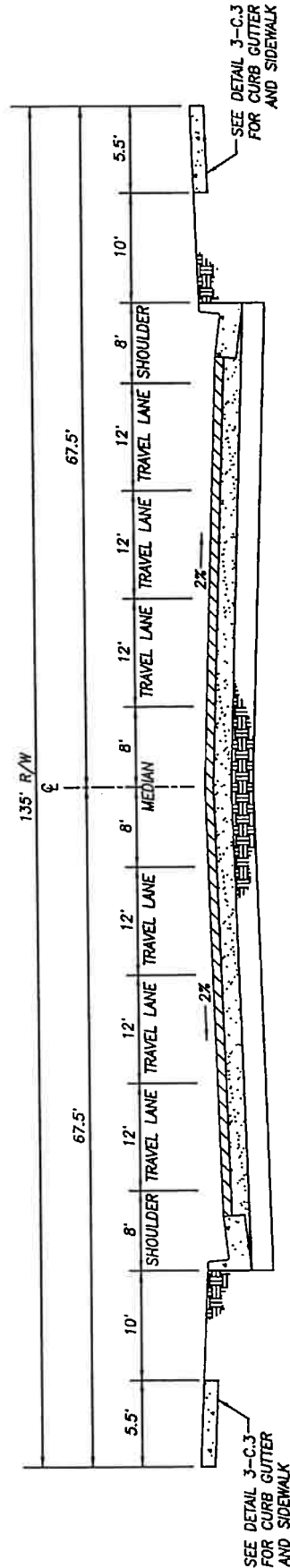
APPROVED BY: [Signature]
DATE APPROVED: 8/13/87

CITY OF HUGHSON

STANDARD DETAIL
3-ST.1.2



SIX LANE EXPRESSWAY WITH LIMITED 124 FOOT ROW



SIX LANE EXPRESSWAY WITH 135 FOOT ROW



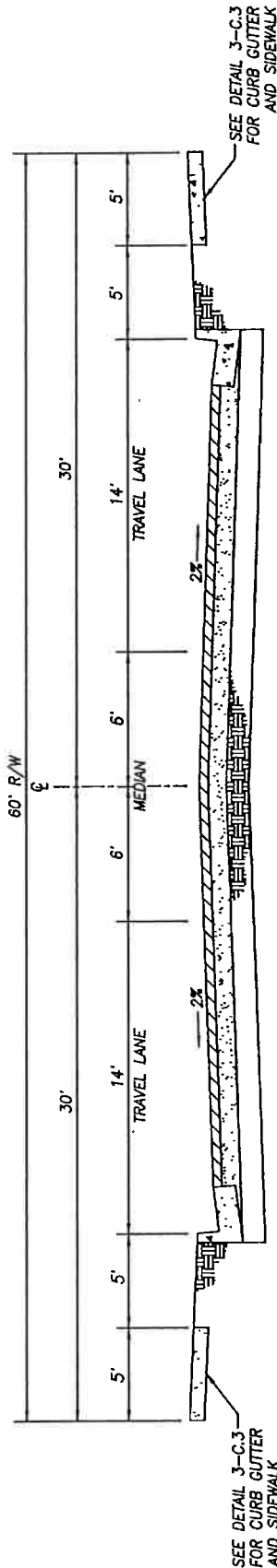
TYPICAL STREET SECTIONS EXPRESSWAY - SIX LANE

DRAWN BY: C.V.
 CHECKED BY: P.K.
 SCALE: NONE
 DATE: 7/07

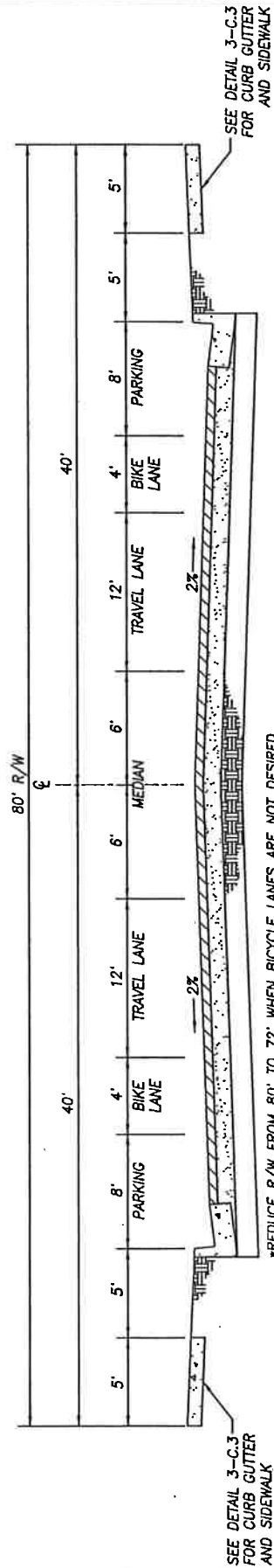
APPROVED BY: 
 DATE APPROVED: 8/13/07

CITY OF HUGHSON

STANDARD DETAIL
 3-ST.2.2



TWO LANE ARTERIAL WITH NO PARKING
(LIMITED RIGHT-OF-WAY)



TWO LANE ARTERIAL WITH PARKING



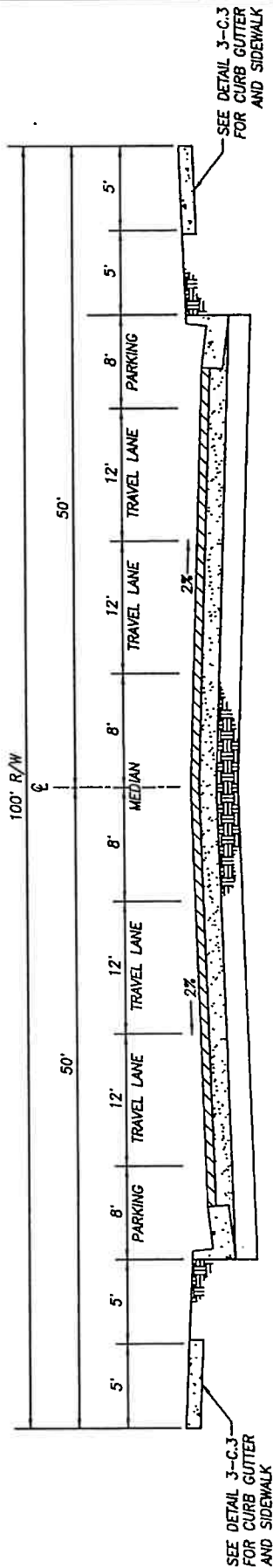
TYPICAL STREET SECTIONS ARTERIAL - TWO LANE NO PARKING & PARKING

DRAWN BY: C.V.
CHECKED BY: P.K.
SCALE: NONE
DATE: 7/07

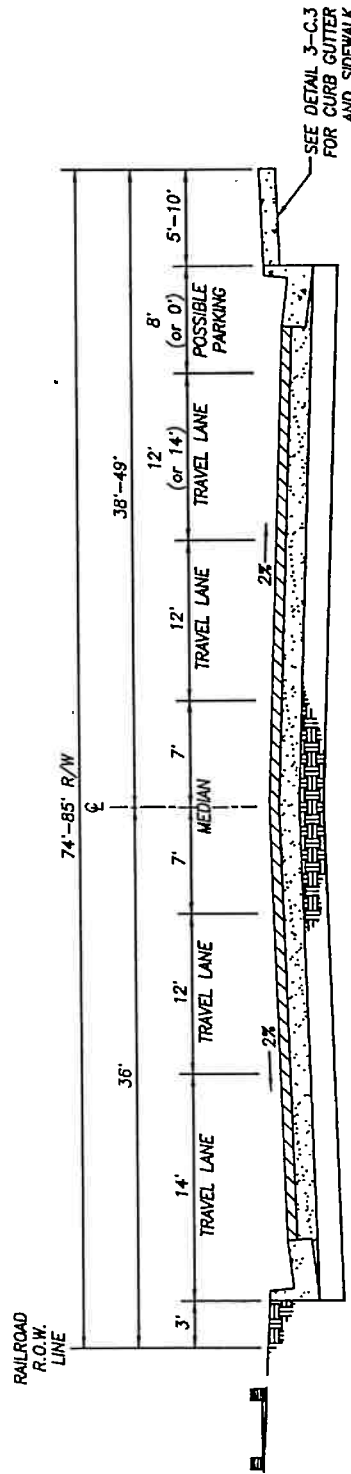
APPROVED BY: *[Signature]*
DATE APPROVED: 8/13/17

CITY OF HUGHSON

STANDARD DETAIL
3-ST.3.1



FOUR LANE ARTERIAL WITH PARKING




SANTA FE AVENUE CONSTRAINED ARTERIAL CORRIDOR



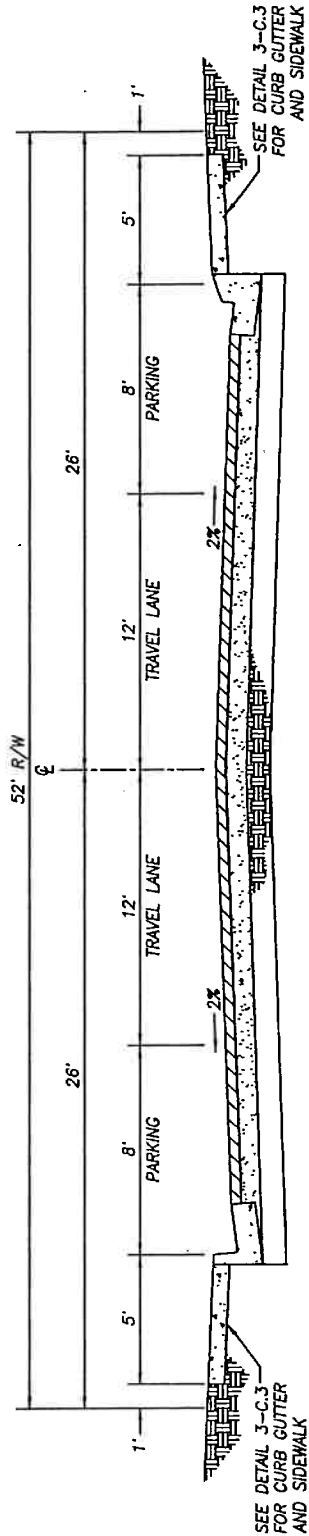
TYPICAL STREET SECTIONS ARTERIAL - FOUR LANE & CONSTRAINED CORRIDOR

DRAWN BY: C.V.
CHECKED BY: P.K.
SCALE: NONE
DATE: 7/07

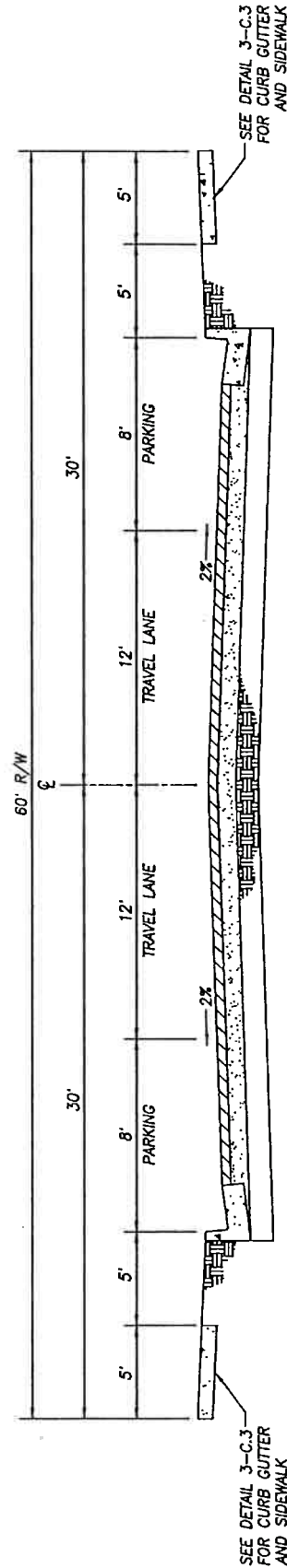
APPROVED BY: 
DATE APPROVED: 8/13/07

CITY OF HUGHSON

STANDARD DETAIL
3-ST.3.2



LOCAL RESIDENTIAL STREET WITH ROLLOVER CURB OR VERTICAL CURB



LOCAL COMMERCIAL STREET



TYPICAL STREET SECTIONS LOCAL - RESIDENTIAL & LOCAL

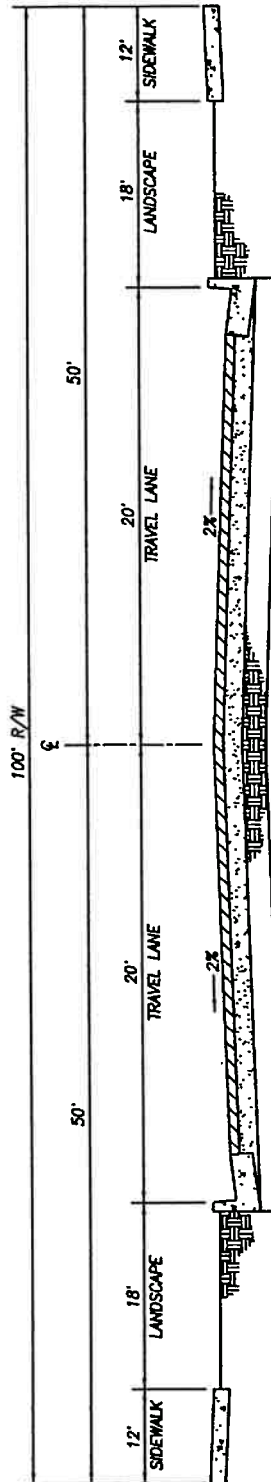
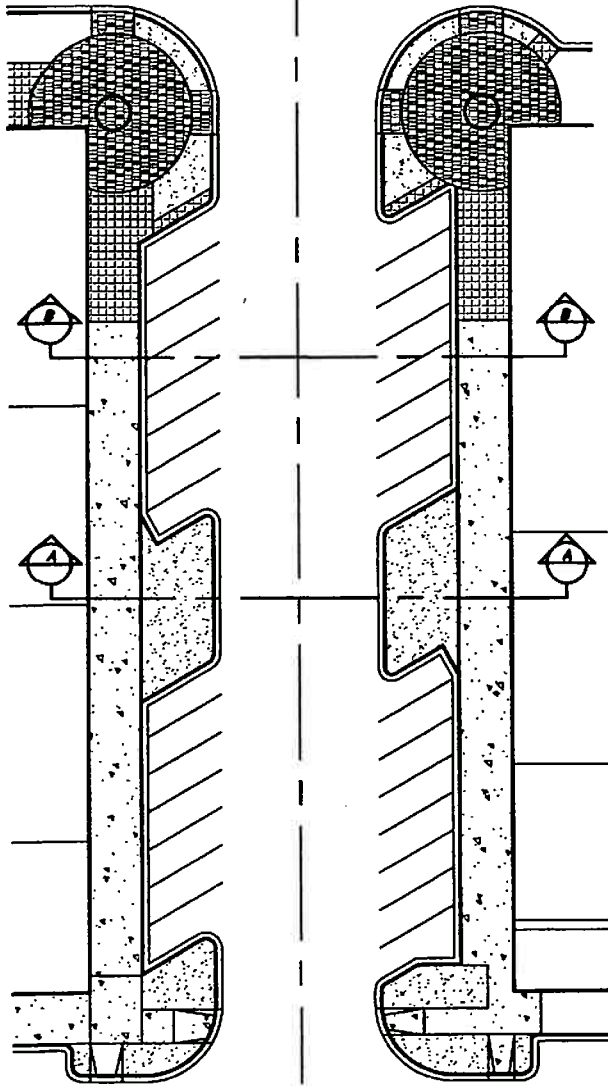
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CHECKED BY: P.K.
SCALE: NONE
DATE: 7/07

APPROVED BY:
DATE APPROVED: 8/13/07

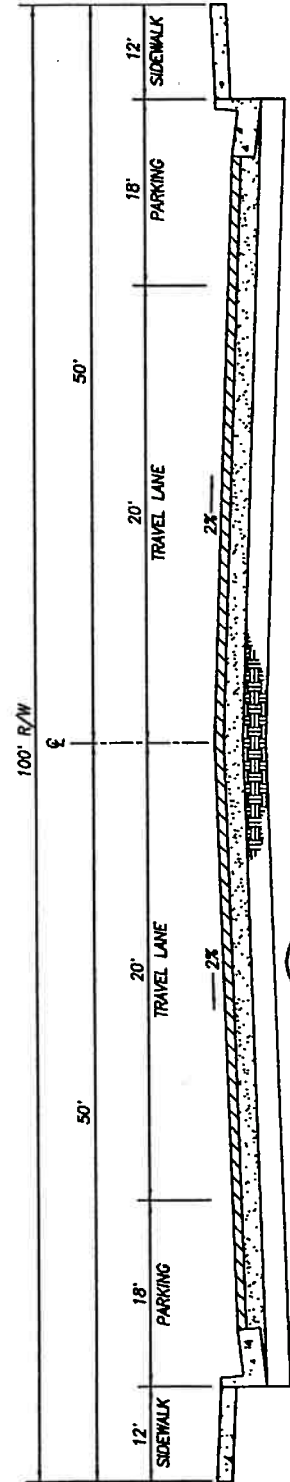
CITY OF HUGHSON

STANDARD DETAIL

3-ST.3.3



A STREET SECTION
NO SCALE



B STREET SECTION
NO SCALE



TYPICAL STREET SECTIONS

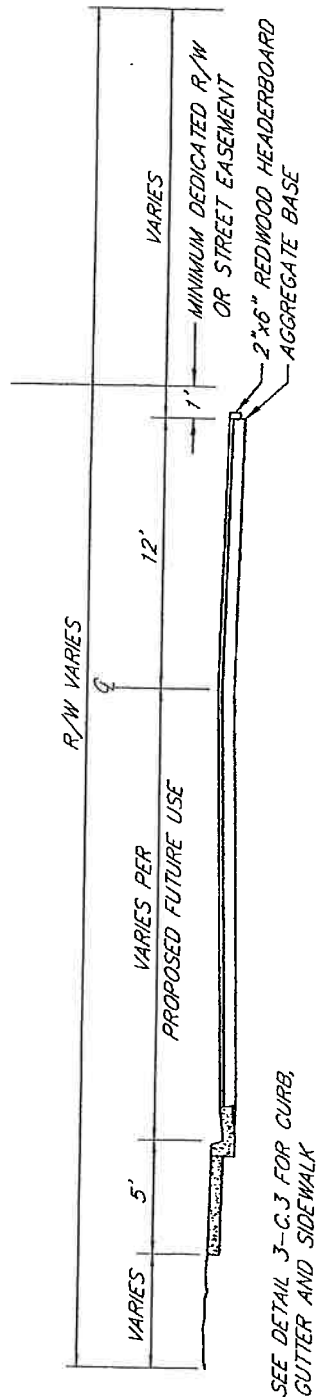
CENTRAL TRAFFIC DISTRICT - STREET PLAN & PROFILE

DRAWN BY: C.V.
CHECKED BY: P.K.
SCALE: NONE
DATE: 7/07

APPROVED BY: [Signature]
DATE APPROVED: 8/15/17

CITY OF HUGHSON

STANDARD DETAIL
3-ST.3.4



SEE DETAIL 3-C.3 FOR CURB,
GUTTER AND SIDEWALK

PARTIAL WIDTH STREET IMPROVEMENTS



PARTIAL WIDTH STREET IMPROVEMENTS

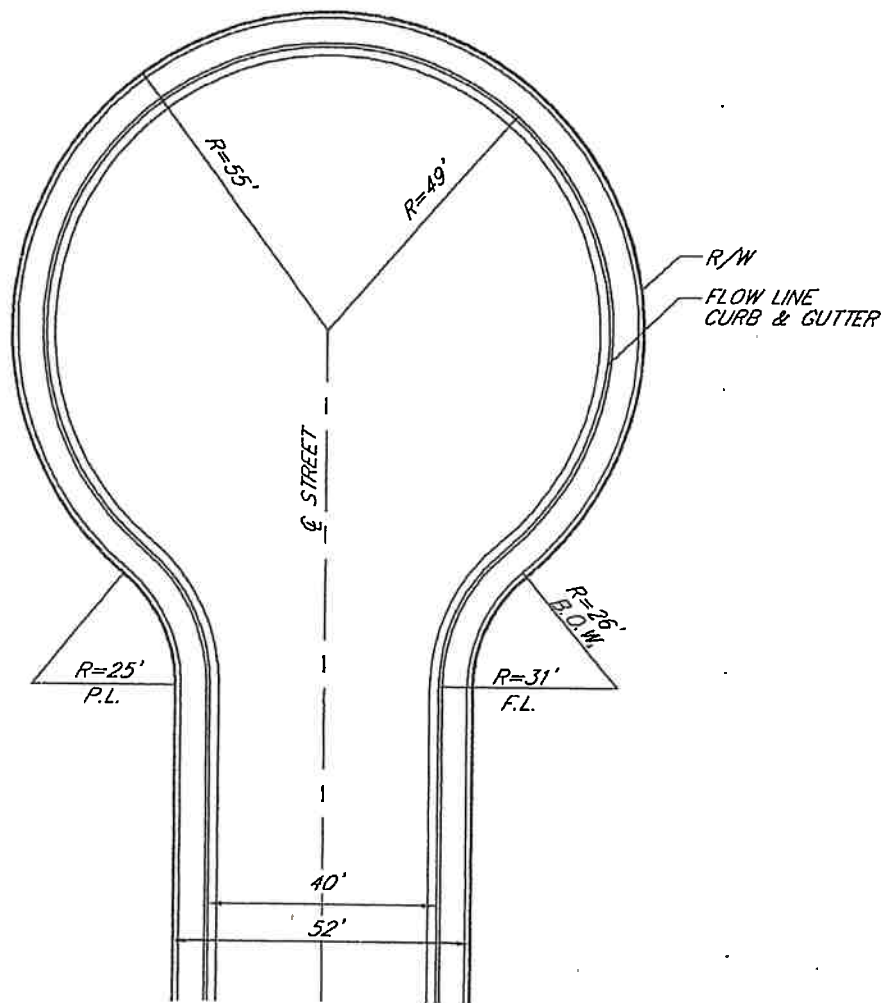
DRAWN BY: A.D.R.
CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

APPROVED BY: *[Signature]*
DATE APPROVED: 12/11/04


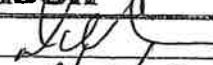
CITY OF HUGHSON

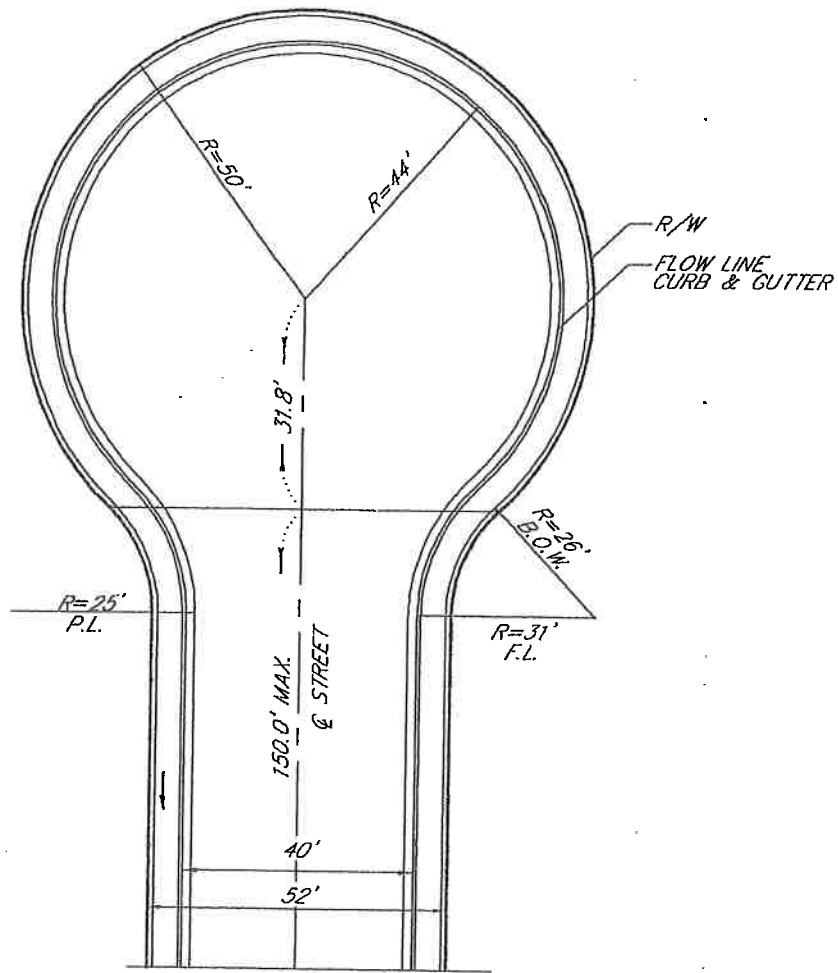
STANDARD DETAIL

3-ST.4



STANDARD CUL-DE-SAC, MAXIMUM DEPTH 500'
FROM INTERSECTING STREET RIGHT-OF-WAY
TO POINT OF REVERSE CURVE AT BULB.

	CUL-DE-SAC DESIGN REQUIREMENTS	DRAWN BY: A.D.R. CHECKED BY: R.H.H. SCALE: NONE DATE: 1/04
APPROVED BY:  DATE APPROVED: 8/13/17	CITY OF HUGHSON	STANDARD DETAIL 3-ST.5



STANDARD FOR CUL-DE-SAC 150' AND LESS
IN LENGTH TO POINT OF REVERSE CURVE.



150' CUL-DE-SAC DESIGN REQUIREMENTS

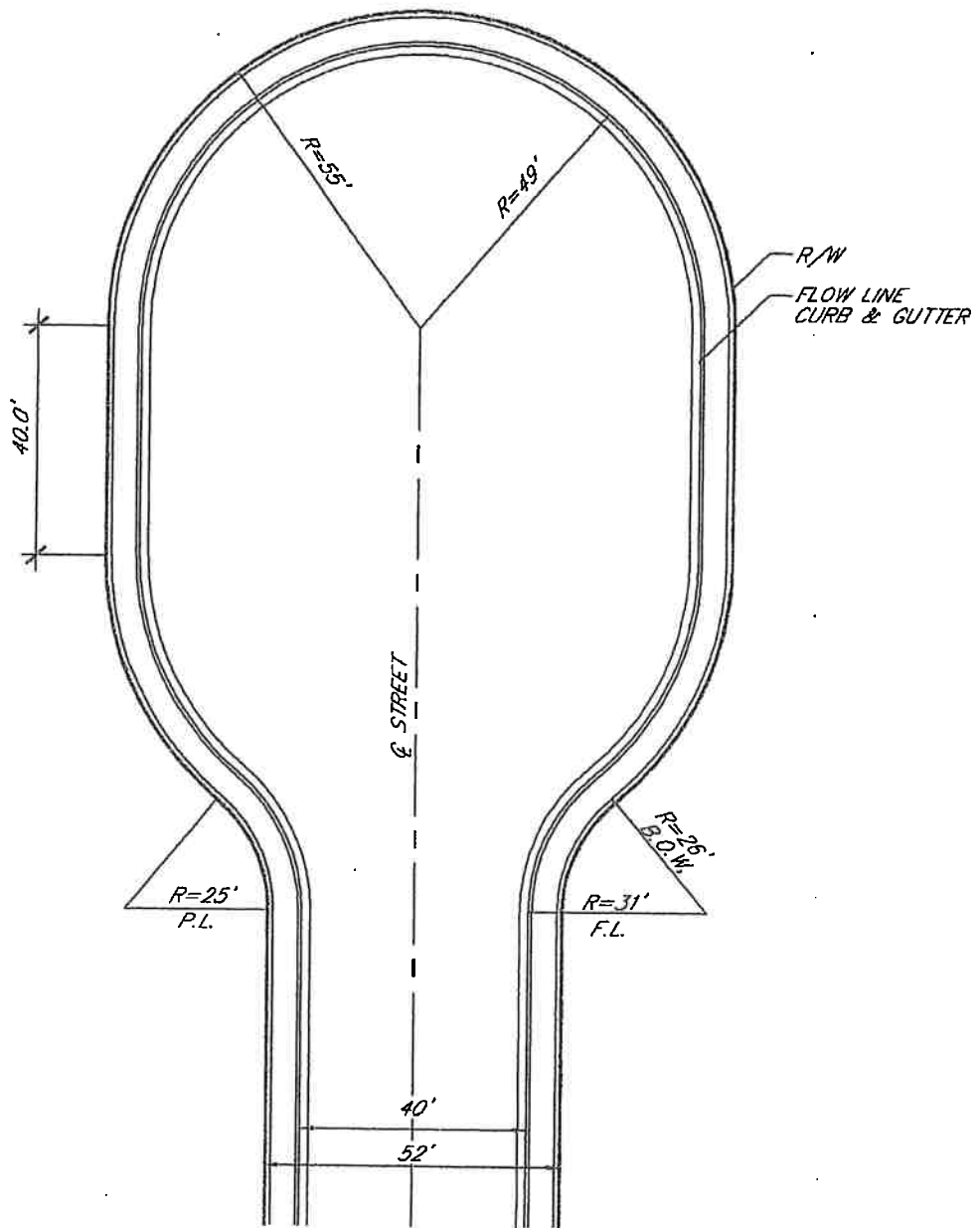
DRAWN BY: A.D.R.
CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

APPROVED BY: *[Signature]*
DATE APPROVED: 8/13/07

CITY OF HUGHSON

STANDARD DETAIL

3-ST.5a



STANDARD CUL-DE-SAC, MAXIMUM DEPTH 500'
FROM INTERSECTING STREET RIGHT-OF-WAY
TO POINT OF REVERSE CURVE AT BULB.



CUL-DE-SAC DESIGN REQUIREMENTS

DRAWN BY: A.D.R.
CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

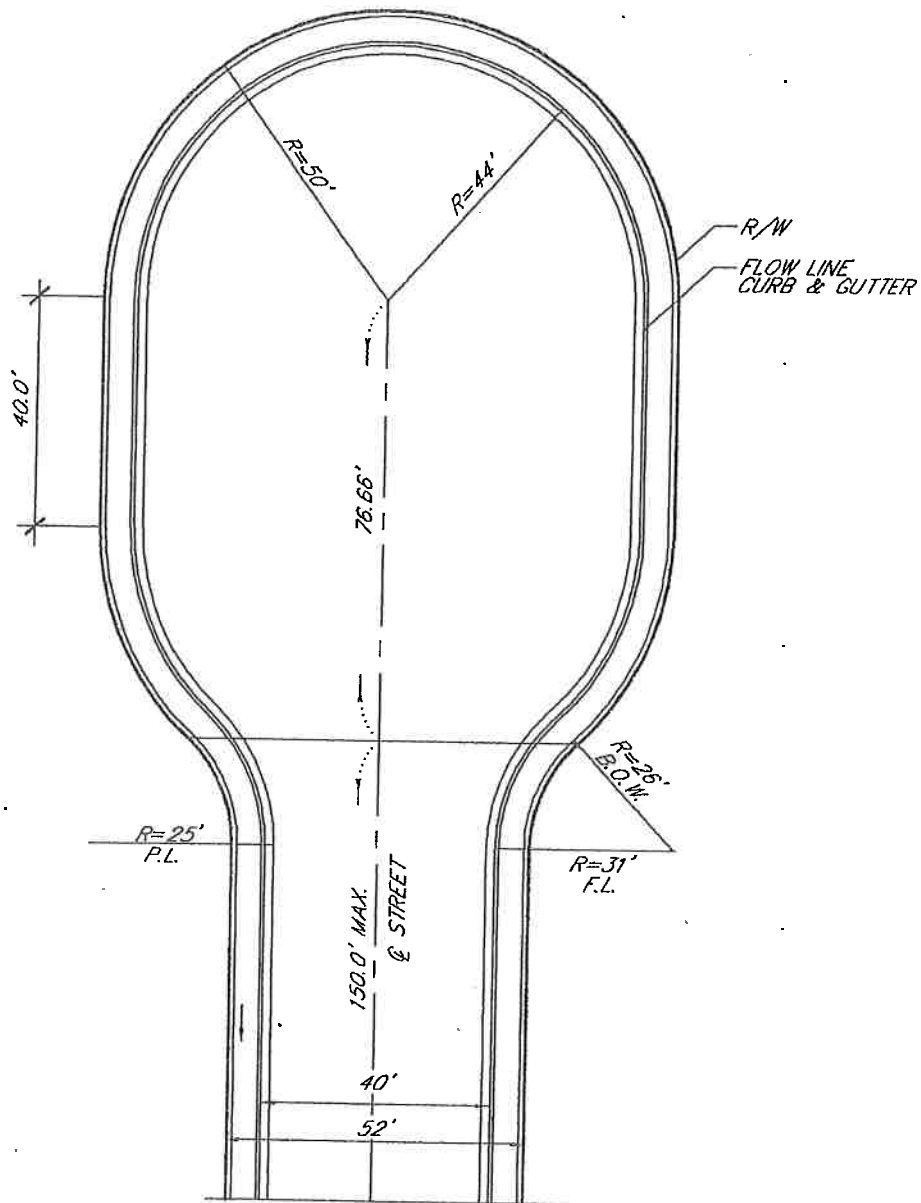
APPROVED BY: *Jcl*

DATE APPROVED: 8/15/07

CITY OF HUGHSON

STANDARD DETAIL

3-ST.5b



STANDARD FOR CUL-DE-SAC 150' AND LESS
IN LENGTH TO POINT OF REVERSE CURVE.



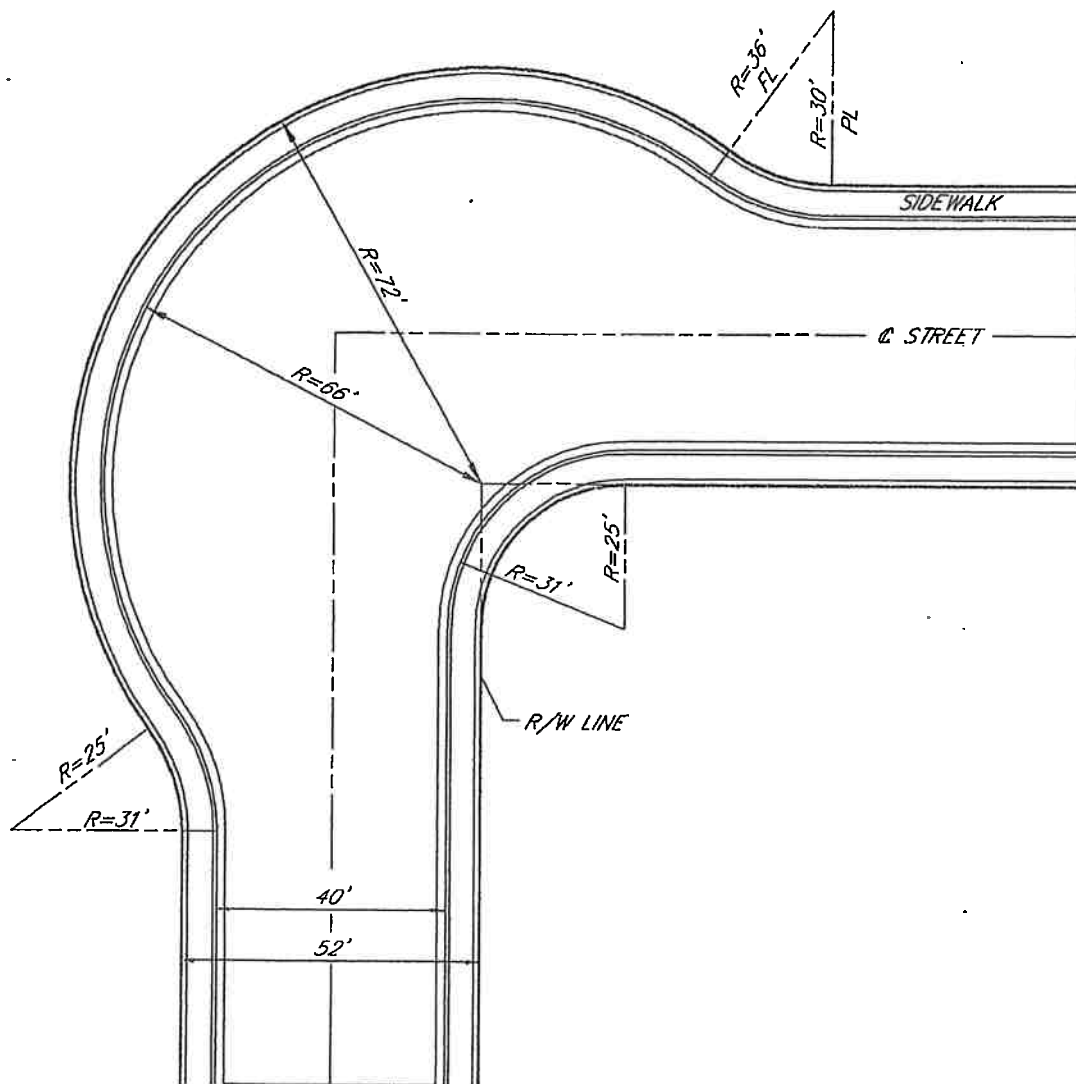
150' CUL-DE-SAC DESIGN REQUIREMENTS

DRAWN BY: A.D.R.
CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

APPROVED BY: *[Signature]*
DATE APPROVED: 8/15/07

CITY OF HUGHSON

STANDARD DETAIL
3-ST.5c



INTERSECTION BULB

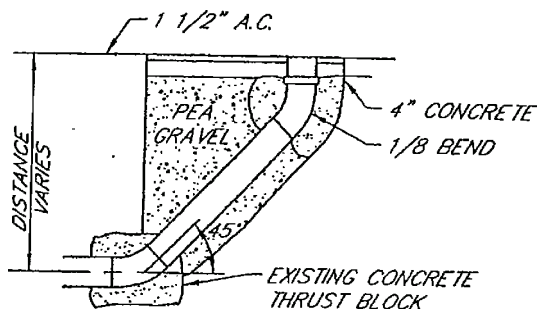
DRAWN BY: A.D.R.
 CHECKED BY: R.H.H.
 SCALE: NONE
 DATE: 1/04

APPROVED BY: *[Signature]*
 DATE APPROVED: 8/13/07

CITY OF HUGHSON

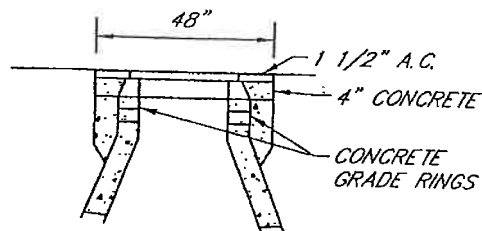
STANDARD DETAIL

3-ST.6



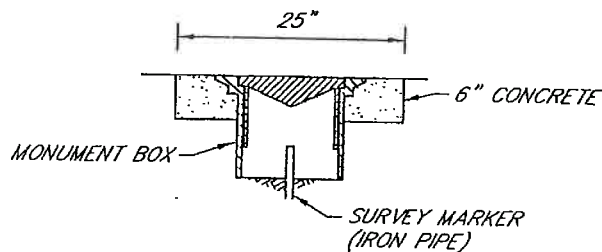
REMOVE CASTING COVER AND ONE JOINT OF PIPE BEFORE EXCAVATION. KEEP SEWER PLUGGED TO KEEP OUT DIRT. RESET TO GRADE AFTER A.C. IS LAID.

LAMPHOLE



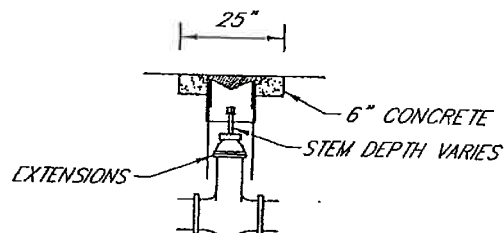
REMOVE CASTING, COVER AND ADJUSTING RINGS TO TOP OF CONCRETE COVER TO KEEP OUT DIRT AND ROCKS WHILE WORKING OVER MANHOLE. IF TOP OF CONE IS LOWER THAN BOTTOM OF 6" COMPACTION, THEN CASTING AND RINGS SHALL BE REMOVED BEFORE STARTING 6" COMPACTION.

MANHOLE



REMOVE CASTING, COVER AND NECESSARY EXTENSIONS BELOW COMPACTION. CAUTION SHALL BE TAKEN SO AS NOT TO DISTURB MONUMENT. RESET TO GRADE AFTER A.C. IS LAID.

MONUMENT BOX



REMOVE VALVE BOX AND EXTENSIONS BELOW COMPACTION. PROTECT VALVE STEM WHEN IN CONSTRUCTION AREA. AFTER A.C. IS LAID, CLEAN OUT REMAINING EXTENSIONS SO STEM IS CLEAR OF DIRT, AND RESET TO GRADE.

VALVE BOX



ADJUSTMENT OF FRAMES & COVERS

DRAWN BY: A.D.R.
CHECKED BY: R.H.H.
SCALE: NONE
DATE: 1/04

APPROVED BY: *[Signature]*
DATE APPROVED: 5/1/04

CITY OF HUGHSON

STANDARD DETAIL

3-ST.7

